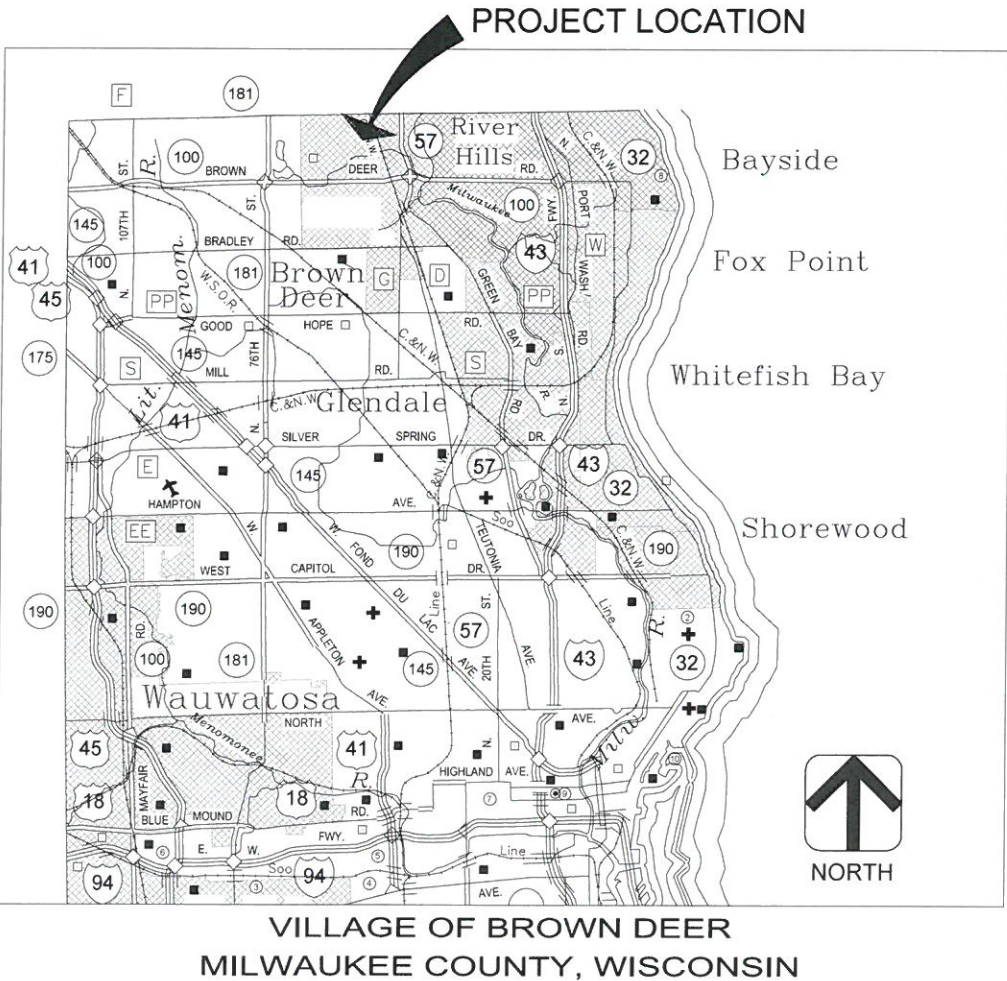
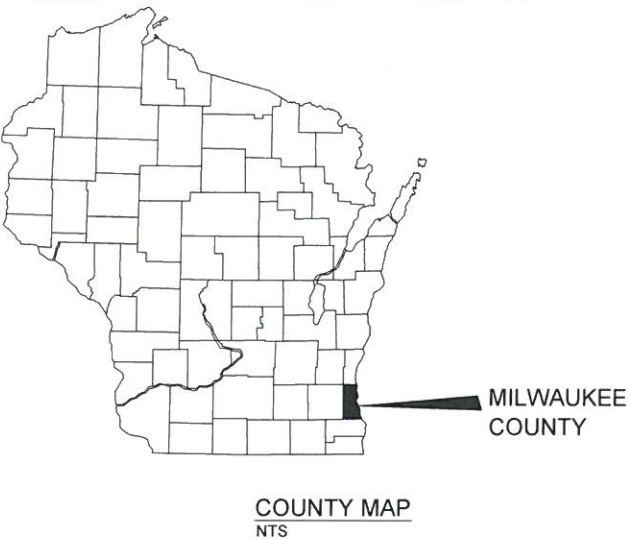


W. FAIRY CHASM ROAD RECONSTRUCTION

VILLAGE OF BROWN DEER

N. 60TH STREET TO N. 51ST STREET

FEBRUARY 2016



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SANITARY SEWER	
WATER MAIN	
STORM SEWER	
UNDERGROUND GAS	
UNDERGROUND ELECTRIC	
UNDERGROUND FIBER OPTIC	
UNDERGROUND TELEPHONE	
UNDERGROUND VIDEO	
OVERHEAD ELECTRIC	
OVERHEAD UTILITY	
UTILITY POLES:	
TELEPHONE POLE	
LIGHT POLE	
CURB AND GUTTER	
HARD SURFACED ROADWAY	<p>ASPHALT, CONCRETE, ETC.</p>
NON-SURFACED ROADWAY	<p>GRAVEL, DIRT, ETC.</p>
SIDEWALK	<p>SIDEWALK</p>
STREET CENTERLINE	
RETAINING WALL	
TREES	<p>DECIDUOUS</p> <p>CONIFEROUS</p>
TREE LINE	
HEDGE OR BUSH LINE	
BUSH / SHRUB	
CONTOURS	<p>100</p> <p>99</p>
DITCH	
FENCE	
RIGHT OF WAY	<p>ROW MARKER</p> <p>IRON PIPE</p>
PROPERTY LINE	<p>IP</p>
SECTION LINE	<p>SECTION CORNER</p> <p>MONUMENT</p>
WETLAND	
SWAMP EDGE	
WATER'S EDGE	
RAILROAD	
BUILDING	<p>OVER/DECK</p>
GUARD RAIL	
ANCHOR	<p>ANCHOR</p>
CONTROL BOX	
METER	
PARKING METER	
POLE	
PULLBOX	
RR SIGNAL FLASHER / BOX	
SIGN	
SPRINKLER HEAD	
TRAFFIC SIGNAL	<p>W/ MAST ARM</p>
VALVE	<p>VLV</p>

The legend is organized into two columns. The left column lists the names of the features, and the right column shows their corresponding graphical symbols. The features include:

- SANITARY SEWER:** Represented by a solid black line with a circular manhole (MH) and a rectangular access box.
- FORCE MAIN:** Represented by a solid black line.
- WATER MAIN:** Represented by a solid black line with a valve (V) and a hydrant (HYD).
- STORM SEWER:** Represented by a dashed black line with a circular manhole (MH) and a rectangular inlet.
- HARD SURFACE ROADWAY:** Represented by two parallel solid black lines.
- NON-SURFACED ROADWAY:** Represented by two parallel dashed black lines.
- CURB AND GUTTER:** Represented by a solid black line with a hatched curb.
- SIDEWALK:** Represented by two parallel solid black lines.
- BASELINE:** Represented by a solid black line with a central point and a distance marker (e.g., 10+00).
- CONTOURS:** Represented by curved lines with elevation markers (e.g., 100, 99).
- FENCE:** Represented by a solid black line with a central point.
- PERMANENT EASEMENT:** Represented by two parallel dashed black lines.
- TEMPORARY EASEMENT:** Represented by two parallel dashed black lines.
- CONSTRUCTION EASEMENT:** Represented by two parallel dashed black lines.
- UTILITY EASEMENT:** Represented by two parallel dashed black lines.
- SETBACK:** Represented by two parallel dashed black lines.
- RETAINING WALL:** Represented by a solid black line with a hatched area.
- POND:** Represented by a solid black line with a central point.
- DITCH:** Represented by a solid black line with a central point.
- SILT FENCE:** Represented by a solid black line with a central point and a distance marker (e.g., SF).
- EROSION BALES:** Represented by a solid black line with a central point.
- EROSION MAT:** Represented by a solid black line with a central point.
- EROSION LOG:** Represented by a solid black line with a central point.
- SAWCUT:** Represented by a solid black line with a central point.
- GUARD RAIL:** Represented by a solid black line with a central point.
- TOP OF CUT:** Represented by a solid black line with a central point.
- TOP OF FILL:** Represented by a solid black line with a central point.
- SPOT ELEVATION:** Represented by a solid black line with a central point and a distance marker (e.g., +800.80).
- CONTROL POINT:** Represented by a triangle symbol with the text "CP1".
- BENCHMARK:** Represented by a circle symbol with the text "BM100 1347.60".



A/E	ARCHITECT/ENGINEER	DEG °	DEGREE	HSE	HOUSE	PC	POINT OF CURVE	TOC	TOP OF CURB
AL	ALUMINUM	DI	DUCTILE IRON	HW	HOT WATER	PE	PRIVATE ENTRANCE	TEL	TELEPHONE
ALT/	ALTERNATE	DIA	DIAMETER	HWR	HOT WATER RETURN	PED	PEDESTAL	THK	THICKNESS
APPROX	APPROXIMATE	DIM	DIMENSION	HYD	HYDRANT	PERF	PERFORATE	THRU	THROUGH
ASPH	ASPHALT	DR	DOOR			PI	POINT OF INTERSECTION	TYP	TYPICAL
AUTO	AUTOMATIC	DTL	DETAIL	ID	INSIDE DIAMETER	P/L R	PROPERTY LINE		
AVE	AVENUE	DW	DRIVEWAY	IN (")	INCHES	PP	POWER POLE	UNEXC	UNEXCAVATED
@	AT	DWG	DRAWING	INF	INFLUENT	PSF	POUNDS PER SQUARE FOOT	USH	UNITED STATES HIGHWAY
				INL	INLET	PSI	POUNDS PER SQUARE INCH		
BOC	BACK OF CURB	E	EAST	INSUL	INSULATION	PT	POINT OF TANGENCY	V	VALVE
BIT	BITUMINOUS	EA	EACH	INV	INVERT	PVC	POLYVINYL CHLORIDE	V&B	VALVE & BOX
BLDG	BUILDING	EFF	EFFLUENT	IP	IRON PIPE	PVMT	PAVEMENT	VAR	VARIABLE
BLK	BLOCK	EL	ELEVATION			%	PERCENT	VC	VERTICAL CURVE
BLVD	BOULEVARD	ELB	ELBOW	JT	JOINT				
BM	BENCHMARK	ELEC	ELECTRICAL			QTY	QUANTITY	W	WEST
BO	BREAKOFF	EQ	EQUAL	LC	LENGTH OF CURVE	RAD	RADIUS	W/	WITH
BOW	BACK OF SIDEWALK	ER	END OF RADIUS	LF	LINEAL FEET	RCP	REINFORCED CONCRETE PIPE	W/O	WITHOUT
BTM	BOTTOM	EW	EACH WAY	LP	LIGHT POLE	RD	ROAD	WM	WATER MAIN
B/L	BASELINE	EX	EXISTING	LS	LUMP SUM	RDWY	ROADWAY	WS	WATER SURFACE
		EXP	EXPANSION	LT	LEFT	RED	REDUCER	WTP	WATER TREATMENT PLANT
C&G	CURB AND GUTTER			MAS	MASONRY	REF	REFERENCE	WTR	WATER
CB	CATCH BASIN	FF	FINISHED FOOR	MAX	MAXIMUM	REQD	REQUIRED	WWF	WELDED WIRE FABRIC
CF	CUBIC FOOT	FD	FLOOR DRAIN	MFG	MANUFACTURER	REV	REVISED	WWW	WOVEN WIRE MESH
CI	CAST IRON	FDN	FOUNDATION	MH	MANHOLE	RR	RAILROAD	WWTP	WASTEWATER TREATMENT PLANT
C/L	CENTERLINE	FERT	FERTILIZER	MIN	MINIMUM	RT	RIGHT		
CHL	CHLORINE	F-F	FACE TO FACE	MISC	MISCELLANEOUS	RW	RIGHT OF WAY		
CMP	CORRUGATED METAL PIPE	FIN	FINISHED	MJ	MECHANICAL JOINT				
CMU	CONCRETE MASONRY UNIT	FL	FLOWLINE	MP	MID POINT	S	SOUTH		
CO	CLEANOUT	FLG	FLANGED			SAN	SANITARY		
CONC	CONCRETE	FM	FORCE MAIN	N	NORTH	SAMH	SANITARY MANHOLE		
CP	CONTROL POINT	FT (')	FOOT	NO #	NUMBER	SCH	SCHEDULE		
CPLG	COUPLING	FTG	FOOTING	NOM	NOMINAL	SF	SQUARE FOOT		
CSP	CORRUGATED STEEL PIPE	FV	FIELD VERIFY	NPW	NON-POTABLE WATER	SHT	SHEET		
CTH	COUNTY TRUNK HIGHWAY			NTS	NOT TO SCALE	SL	SLOPE		
CULV	CULVERT	G	GAS			SPEC	SPECIFICATION		
CV	CHECK VALVE	GAGE	GAGE			SQ	SQUARE		
CW	COLD WATER	GALV	GALVANIZED	OC	ON CENTER	SS	STAINLESS STEEL		
CY	CUBIC YARD	GAR	GARAGE	OD	OUTSIDE DIAMETER	SSMH	STORM SEWER MANHOLE		
		GEN	GENERAL	OH	OVERHEAD	ST	STREET		
		GRD	GRADE, GROUND			STD	STANDARD		
		GV	GATE VALVE			STH	STATE TRUNK HIGHWAY		
		GRAV	GRAVEL			STL	STEEL		
		GW	GROUNDWATER			STM	STORM		
						SW	SIDEWALK		
						SY	SQUARE YARD		

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W. FAIRY CHASM ROAD RECONSTRUCTION
N. 60TH STREET TO N. 51ST STREET
VILLAGE OF BROWN DEER



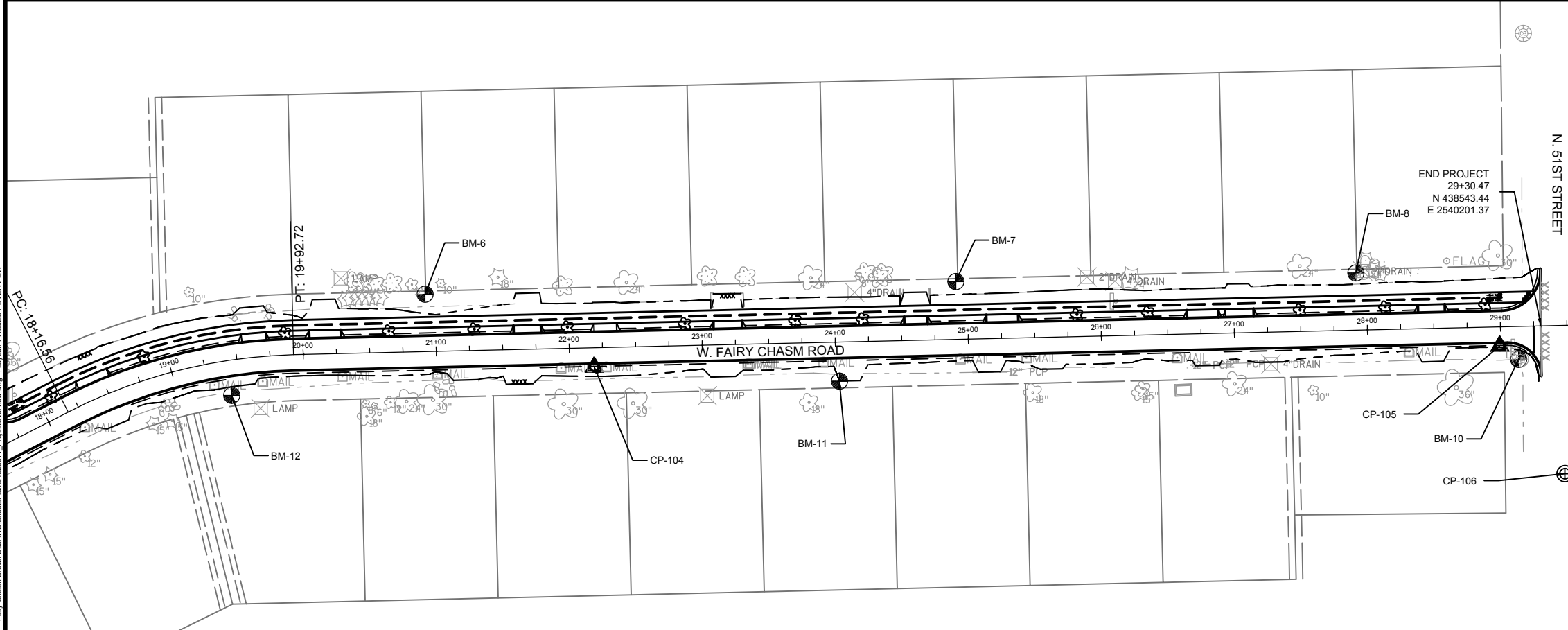
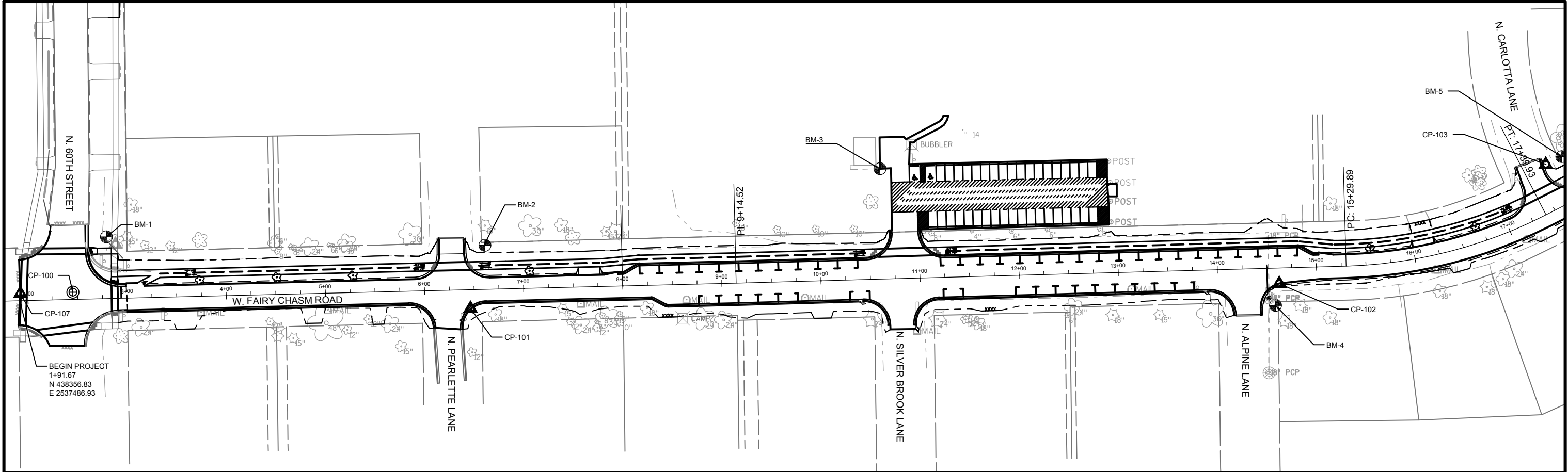
EROSION CONTROL NOTES:

1. THE CONTRACTOR SHALL NOTIFY THE VILLAGE OF BROWN DEER AT LEAST 2 DAYS PRIOR TO THE START OF SOIL DISTURBING ACTIVITIES.
2. EROSION CONTROL MEASURES SHALL BE MAINTAINED THROUGHOUT THE DURATION OF CONSTRUCTION UNTIL THE SITE IS STABILIZED BY VEGETATION OR OTHER APPROVED MEANS.
3. ALL ACTIVITIES SHALL BE CONDUCTED IN A LOGICAL SEQUENCE AS TO MINIMIZE THE AMOUNT OF BARE SOIL EXPOSED AT ANY ONE TIME. MAINTAIN EXISTING VEGETATION AS LONG AS POSSIBLE.
4. DITCH CHECKS SHALL BE PLACED WHERE SHOWN IN EXISTING DITCHES PRIOR TO CONSTRUCTION AND IMMEDIATELY FOLLOWING GRADING OF NEW DITCHES OR INSTALLATION OF NEW OUTFALLS.
5. OFF-SITE SEDIMENT DEPOSITS OCCURRING AS A RESULT OF A STORM EVENT SHALL BE CLEANED UP BY THE END OF THE NEXT WORK DAY. ALL OFF-SITE SEDIMENT DEPOSITS OCCURRING AS A RESULT OF CONSTRUCTION ACTIVITIES, INCLUDING SOIL TRACKED BY CONSTRUCTION TRAFFIC, SHALL AT A MINIMUM, BE CLEANED BY THE END OF EACH WORK DAY. EXCESSIVE AMOUNTS OF SEDIMENT OR DEBRIS TRACKED ONTO ADJACENT STREETS SHALL BE CLEANED IMMEDIATELY. FINE SEDIMENT ACCUMULATIONS SHALL BE CLEANED FROM ADJACENT STREETS BY THE USE OF MECHANICAL OR MANUAL SWEEPING OPERATIONS ONCE A WEEK AT A MINIMUM AND BEFORE IMMINENT RAIN EVENTS.
6. ALL SEDIMENT-LADEN WATER PUMPED FROM THE SITE SHALL BE TREATED BY A TEMPORARY SEDIMENT BASIN OR BE FILTERED BY OTHER APPROVED MEANS. WATER SHALL NOT BE DISCHARGED IN A MANNER THAT CAUSES EROSION OF THE SITE OR RECEIVING CHANNELS.
7. DISTURBED GROUND OUTSIDE OF THE EVERYDAY CONSTRUCTION AREA, INCLUDING SOIL STOCKPILES LEFT INACTIVE FOR MORE THAN 10 DAYS, SHALL, AT A MINIMUM, BE TEMPORARILY STABILIZED BY SEEDING/MULCHING OR OTHER METHODS APPROVED BY THE ENGINEER. SILT FENCE SHALL BE PLACED AROUND ALL SOIL STOCK PILES.
8. EROSION CONTROL DEVICES DESTROYED AS A RESULT OF CONSTRUCTION ACTIVITIES SHALL BE REPAIRED BY THE END OF THE WORK DAY.
9. INSPECT ALL EROSION CONTROL MEASURES AT LEAST ONCE A WEEK AND AFTER ANY RAINFALL OF 0.5-INCHES OR MORE AND MAKE NEEDED REPAIRS. RECORD INSPECTIONS AS SPECIFIED.
10. DRAWINGS SHOW GENERAL LOCATION OF EROSION CONTROL DEVICES AND SIGNS. ACTUAL LOCATIONS MAY VARY AS DETERMINED BY ENGINEER IN THE FIELD.

GENERAL PROJECT NOTES:

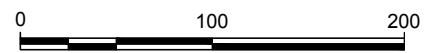
1. INSTALL INLET PROTECTION AND OTHER EROSION CONTROL PRIOR TO BEGINNING ANY CONSTRUCTION ACTIVITY.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING EXISTING PROPERTY IRONS. PROPERTY IRONS REMOVED OR DISTURBED DURING CONSTRUCTION SHALL BE RE-ESTABLISHED BY A REGISTERED LAND SURVEYOR AT THE CONTRACTOR'S EXPENSE.
3. CONTRACTOR SHALL NOT OPERATE CONSTRUCTION EQUIPMENT OUTSIDE OF CONSTRUCTION EASEMENTS OR VILLAGE RIGHT-OF-WAY WITHOUT WRITTEN APPROVAL FROM ENGINEER.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING EXISTING ASPHALT ON STREETS OUTSIDE OF PROPOSED SAW CUTS. ANY DAMAGE DURING CONSTRUCTION UNDER THIS CONTRACT SHALL BE REPAIRED OR REPLACED AT CONTRACTOR'S EXPENSE.
5. ASPHALT OR CONCRETE ROADWAY AND DRIVEWAYS AND CONCRETE CURB & GUTTER SHALL BE SAW CUT PRIOR TO RESTORATION.
6. EXISTING STREET MATERIALS AND THEIR THICKNESS MAY VARY. ALL COST ASSOCIATED WITH REMOVAL OF EXISTING STREET MATERIAL SHALL BE PAID FOR UNDER ROADWAY REMOVALS.
7. CONTRACTOR SHALL PROVIDE RUNOFF BARRIERS PRIOR TO RAIN EVENTS TO PREVENT ROADWAY WASH OUT. INLET PROTECTION SHALL BE PLACED AT ALL INLETS THAT WILL RECEIVE RUNOFF FROM DISTURBED AREAS.
8. LOCATION OF EXISTING UTILITIES MAY VARY FROM THAT SHOWN ON THE PLANS. THE CONTRACTOR SHALL FIELD VERIFY ALL EXISTING UTILITIES TO AVOID POTENTIAL CONFLICTS.
9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING EXISTING UTILITIES AND APPURTENANCES NOT SHOWN TO BE REPLACED. TEMPORARY REMOVAL AND ANY DAMAGE DURING CONSTRUCTION SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
10. WORK ITEMS SHOWN SHALL BE INCIDENTAL TO CONTRACT OTHERWISE INCLUDED IN THE BID FORM.

DES BY	SES	BOOK NO	----	1	2/11/2016	BID DOCUMENTS				W. FAIRY CHASM ROAD RECONSTRUCTION N. 60TH STREET TO N. 51ST STREET VILLAGE OF BROWN DEER	 WALKESHA, WI	GENERAL PROJECT AND EROSION CONTROL NOTES	SHEET NO G-3
DR BY	SES	PROJ NO	24-0266.11										
CHK BY	KKA	DATE	FEB 2016	NO	DATE	REVISION	NO	DATE	REVISION				



BENCHMARK DATA					
POINT NO.	FEATURE	NORTHING	EASTING	ELEVATION	
1	BM	438421.34	2537574.28	98.32	CHISELLED X HYD. SSE TOP FLANGE BOLT
2	BM	438412.89	2537956.03	88.46	CHISELLED X HYD. SW BOTTOM FLANGE BOLT
3	BM	438490.09	2538354.52	81.65	CHISELLED X SE CONCRETE SIDEWALK
4	BM	438352.22	2538753.50	79.03	CHISELLED X HYD. NE BOTTOM FLANGE BOLT
5	BM	438502.40	2539042.16	76.45	CHISELLED X HYD. NW BOTTOM FLANGE BOLT
6	BM	438567.10	2539362.77	77.97	CHISELLED X HYD. SE BOTTOM FLANGE BOLT
7	BM	438576.67	2539761.56	78.62	CHISELLED X HYD. NW BOTTOM FLANGE BOLT
8	BM	438583.13	2540062.28	75.56	CHISELLED X HYD. SW BOTTOM FLANGE BOLT
9	BM	438298.67	2540267.26	76.61	CHISELLED X IN TOP OF HYD. NOZZLE
10	BM	438518.18	2540184.48	73.18	CHISELLED X W. SIDE OF INLET RIM
11	BM	438501.89	2539674.06	79.05	NW COR. CONC. DRIVE ADDRESS 5333
12	BM	438491.15	2539217.33	75.91	NW COR. CONC. DRIVE ADDRESS 5447

PROJECT CONTROL DATA				
POINT NO.	FEATURE	NORTHING	EASTING	DESCRIPTION
100	CP	438365.46	2537540.41	3" IP SECTION CORNER
101	CP	438349.06	2537941.46	12" SPIKE
102	CP	438374.22	2538757.33	12" SPIKE
103	CP	438494.56	2539025.99	12" SPIKE
104	CP	438512.58	2539489.89	12" SPIKE
105	CP	438528.41	2540170.46	12" SPIKE
106	CP	438432.32	2540219.30	BRASS CAP SECTION CORNER
107	CP	438364.04	2537487.48	STEEL "SURVEY MARK" SPIKE
9+14.52	PI	438374.70	2538209.55	
15+29.89	PC	438391.87	2538824.68	
17+39.93	PT	438443.68	2539026.44	
18+16.56	PC	438478.71	2539094.59	
19+92.72	PT	438521.75	2539263.87	



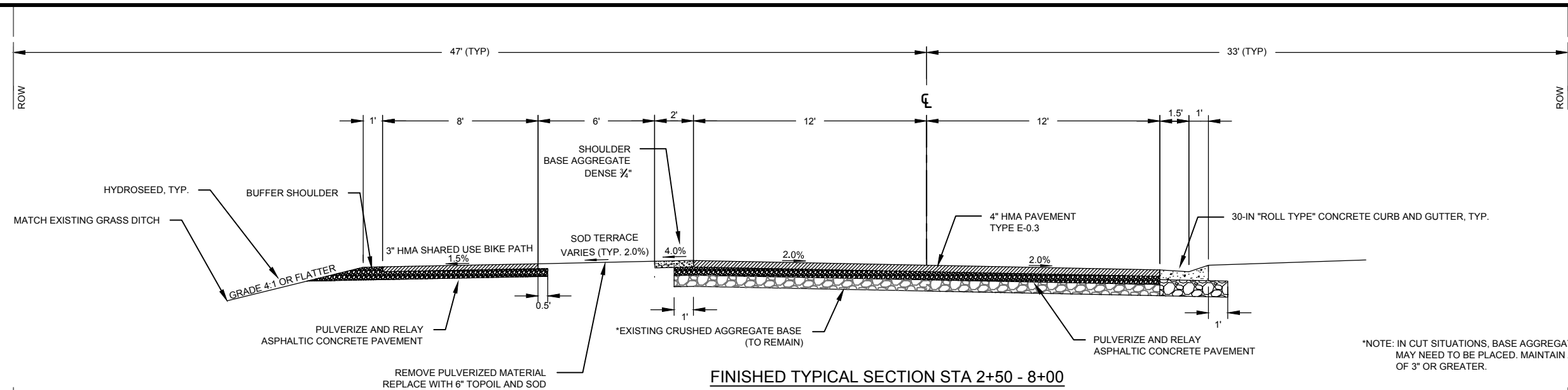
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DR BY	SES	PROJ NO	24-0266.11			#		
CHK BY	KKA	DATE	FEB 2016	NO	DATE	REVISION	NO	DATE

W. FAIRY CHASM ROAD RECONSTRUCTION
N. 60TH STREET TO N. 51ST STREET
VILLAGE OF BROWN DEER



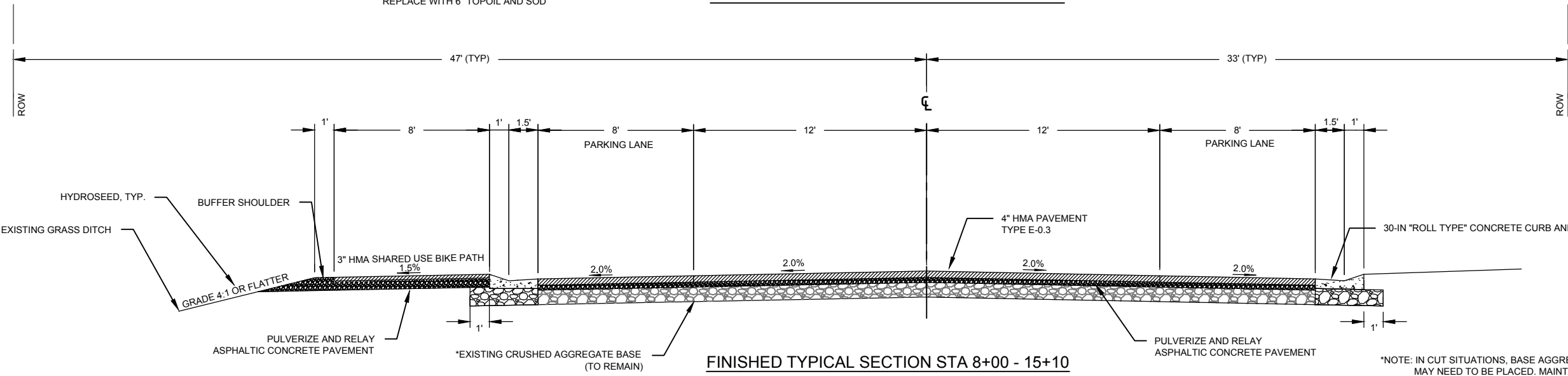
PROJECT OVERVIEW

SHEET NO
G-4



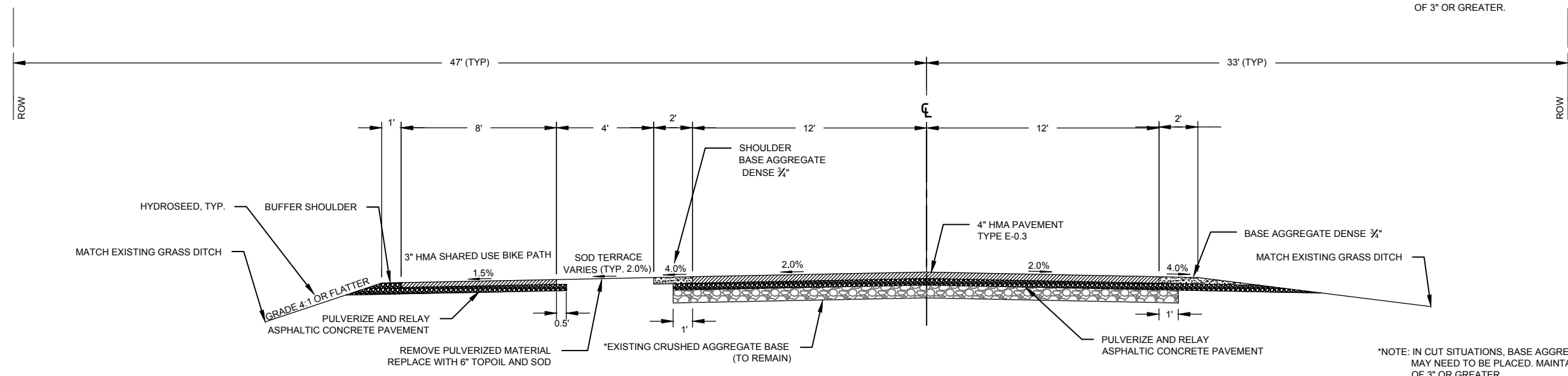
FINISHED TYPICAL SECTION STA 2+50 - 8+00

*NOTE: IN CUT SITUATIONS, BASE AGGREGATE COURSE 1-1/4" AND/OR BREAKER RUN MAY NEED TO BE PLACED. MAINTAIN AN AGGREGATE BASE COURSE THICKNESS OF 3" OR GREATER.



FINISHED TYPICAL SECTION STA 8+00 - 15+10

*NOTE: IN CUT SITUATIONS, BASE AGGREGATE COURSE 1-1/4" AND/OR BREAKER RUN MAY NEED TO BE PLACED. MAINTAIN AN AGGREGATE BASE COURSE THICKNESS OF 3" OR GREATER.



FINISHED TYPICAL SECTION STA 15+10 - 29+50

*NOTE: IN CUT SITUATIONS, BASE AGGREGATE COURSE 1-1/4" AND/OR BREAKER RUN MAY NEED TO BE PLACED. MAINTAIN AN AGGREGATE BASE COURSE THICKNESS OF 3" OR GREATER.

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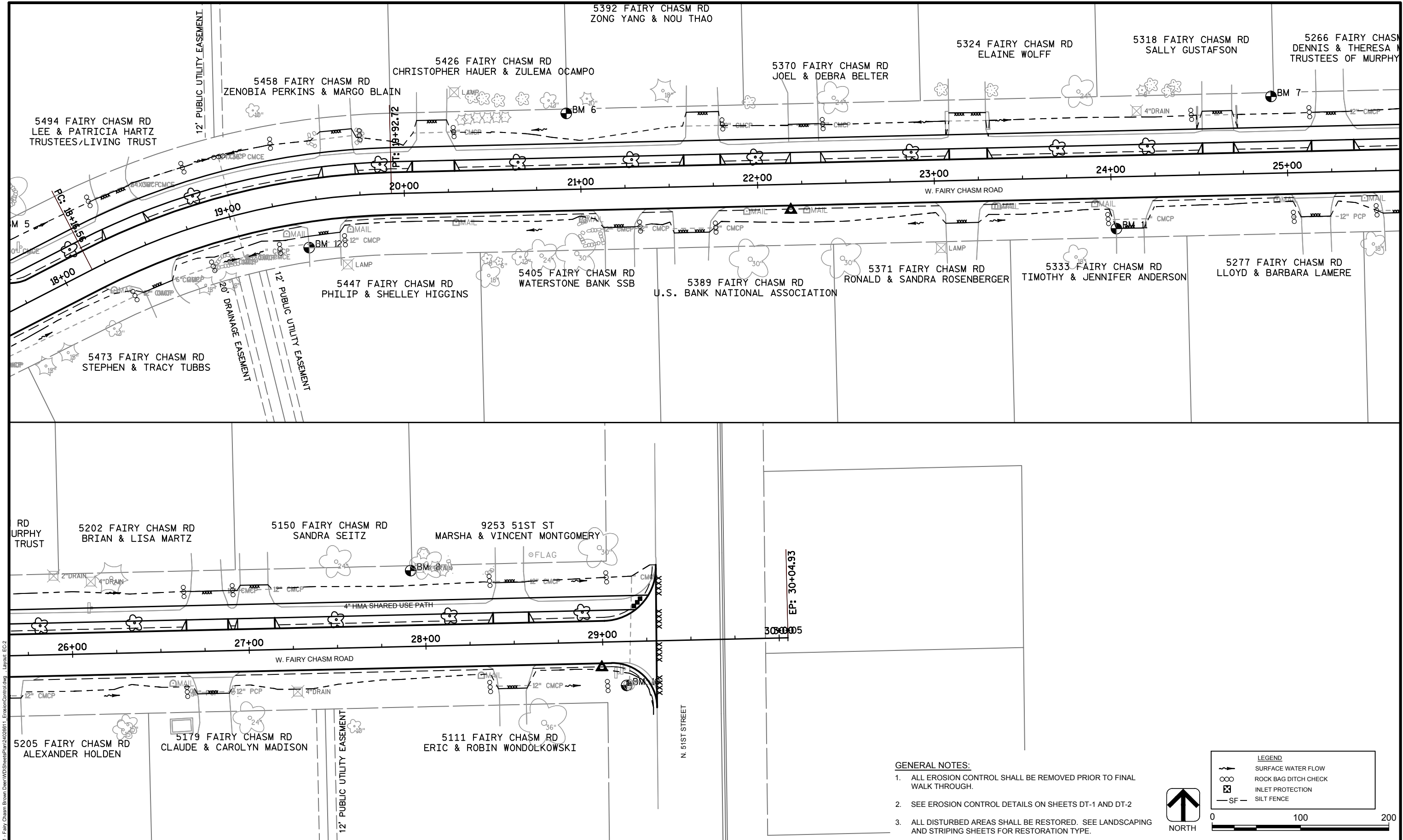
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DR BY	SES	PROJ NO	24-0266.11			#		
CHK BY	KKA	DATE	FEB 2016	NO	DATE	NO	DATE	REVISION

W. FAIRY CHASM ROAD RECONSTRUCTION
N. 60TH STREET TO N. 51ST STREET
VILLAGE OF BROWN DEER



FINISHED TYPICAL SECTIONS

SHEET NO
TS-2



DES BY	SES	BOOK NO	1	2/11/2016	BID DOCUMENTS	#		
DR BY	SES	PROJ NO	#			#		
CHK BY	KKA	DATE	FEB 2016	NO	DATE	REVISION	NO	DATE

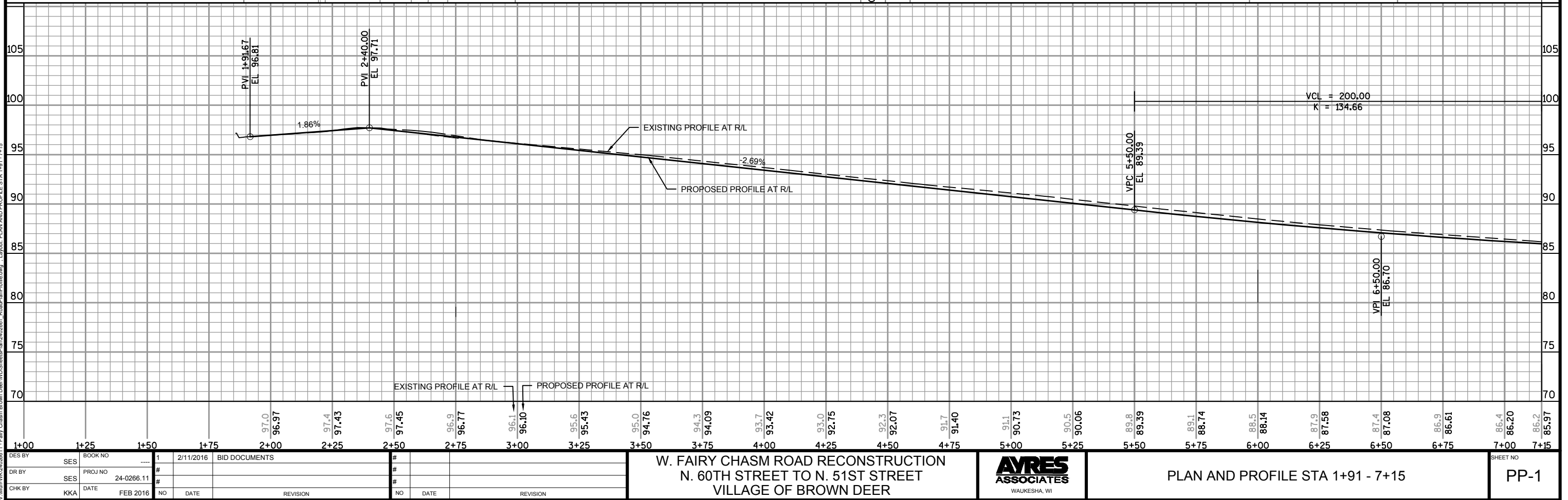
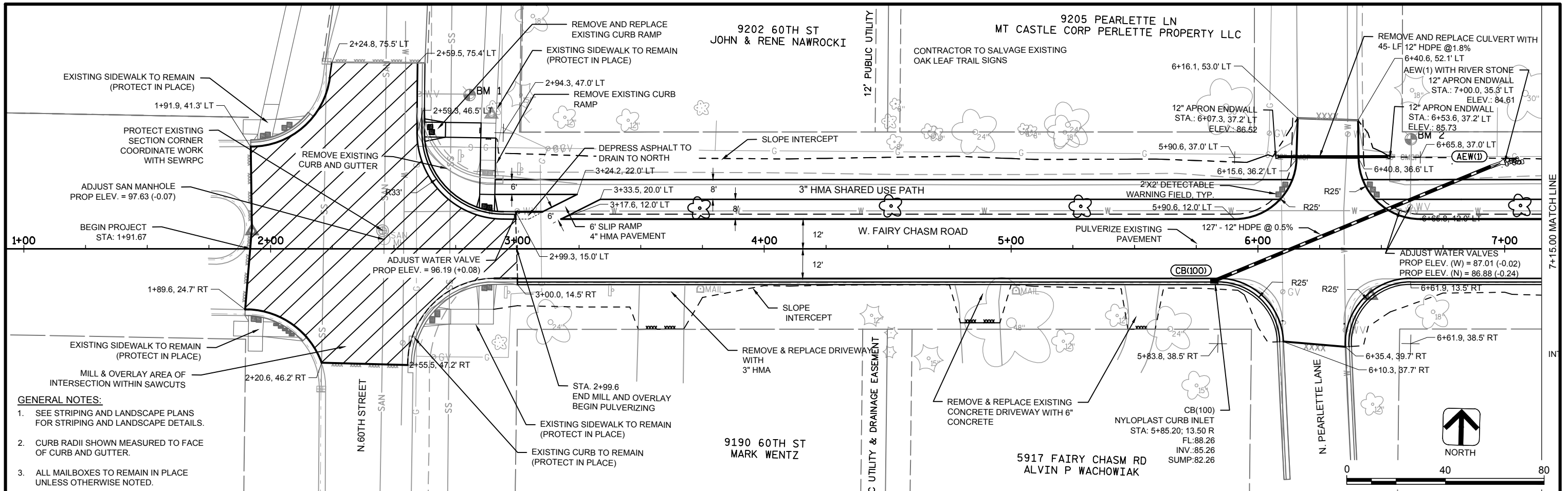
W. FAIRY CHASM ROAD RECONSTRUCTION
N. 60TH STREET TO N. 51ST STREET
VILLAGE OF BROWN DEER

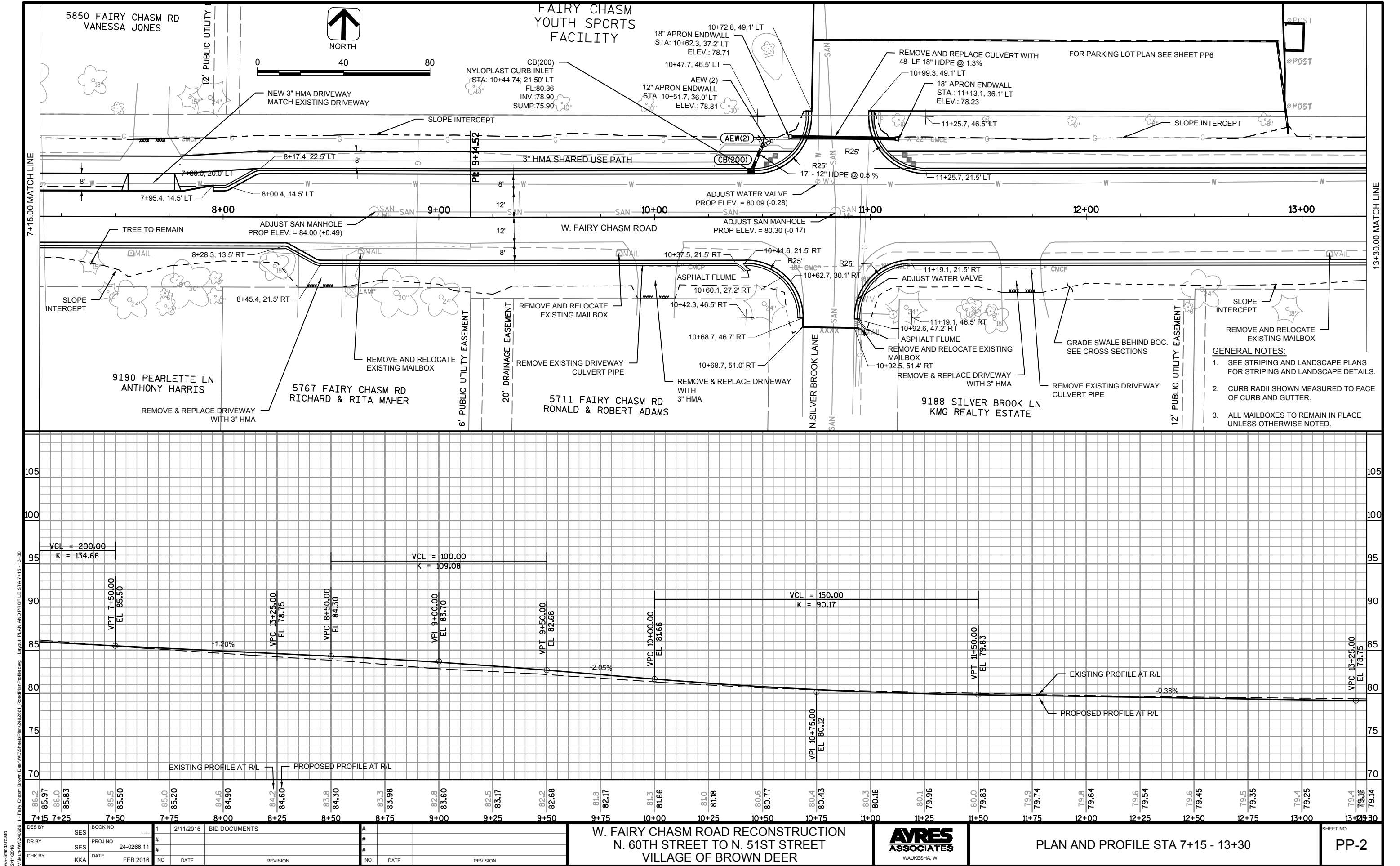


EROSION CONTROL PLAN

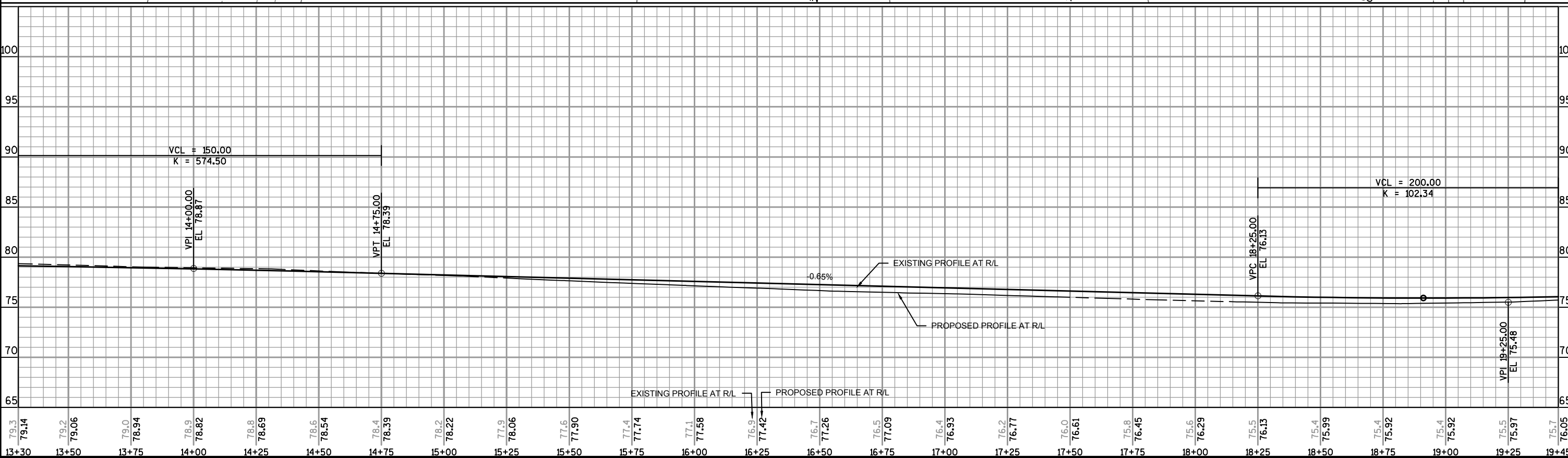
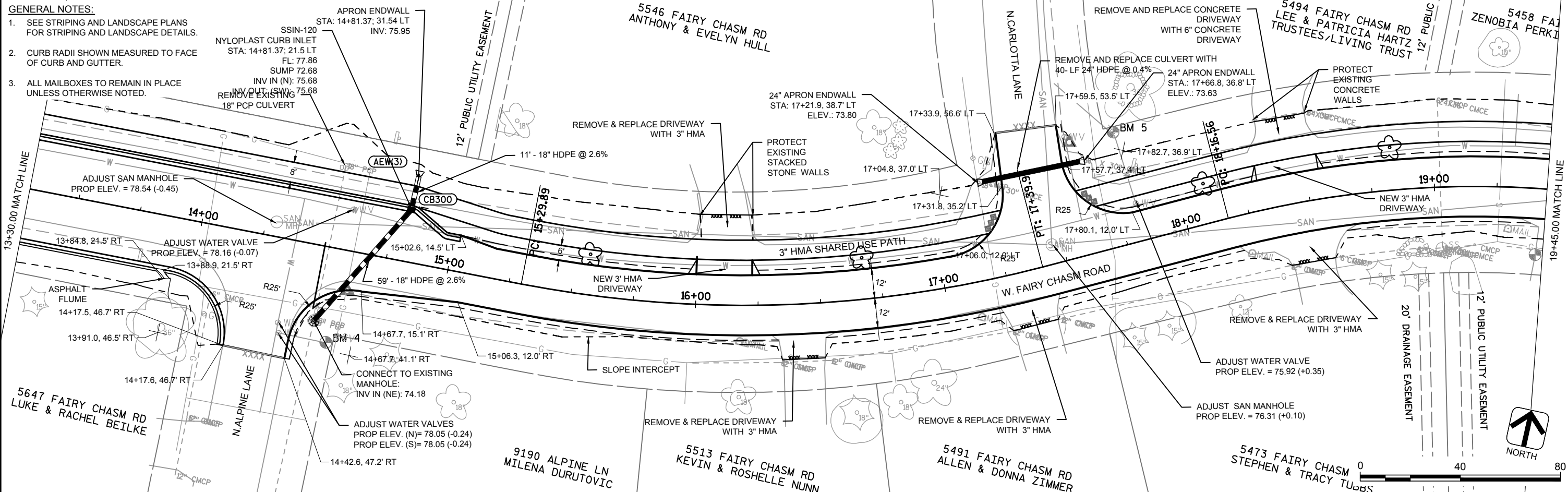
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1. SEE STRIPING AND LANDSCAPE PLANS FOR STRIPING AND LANDSCAPE DETAILS.
2. CURB RADII SHOWN MEASURED TO FACE OF CURB AND GUTTER.
3. ALL MAILBOXES TO REMAIN IN PLACE UNLESS OTHERWISE NOTED.



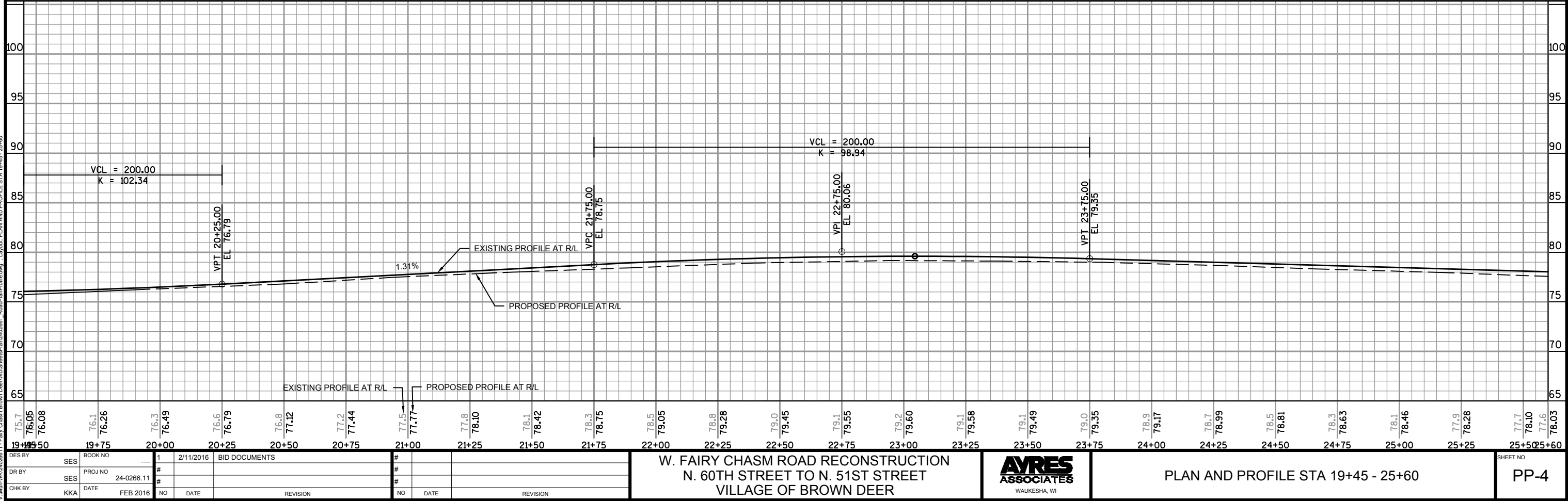
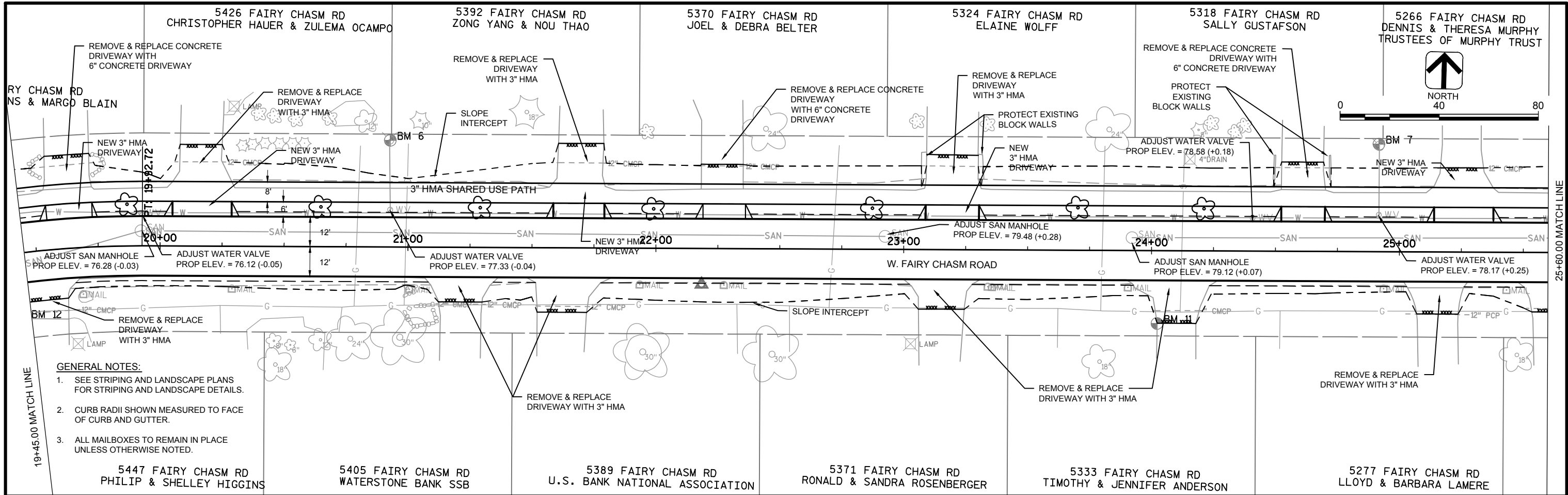
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		24-0266.11	#			#		
CHK BY	KKA	DATE	FEB 2016	NO	DATE	REVISION	NO	DATE
								REVISION

W. FAIRY CHASM ROAD RECONSTRUCTION
N. 60TH STREET TO N. 51ST STREET
VILLAGE OF BROWN DEER



PLAN AND PROFILE STA 13+30 - 19+45

PP-3



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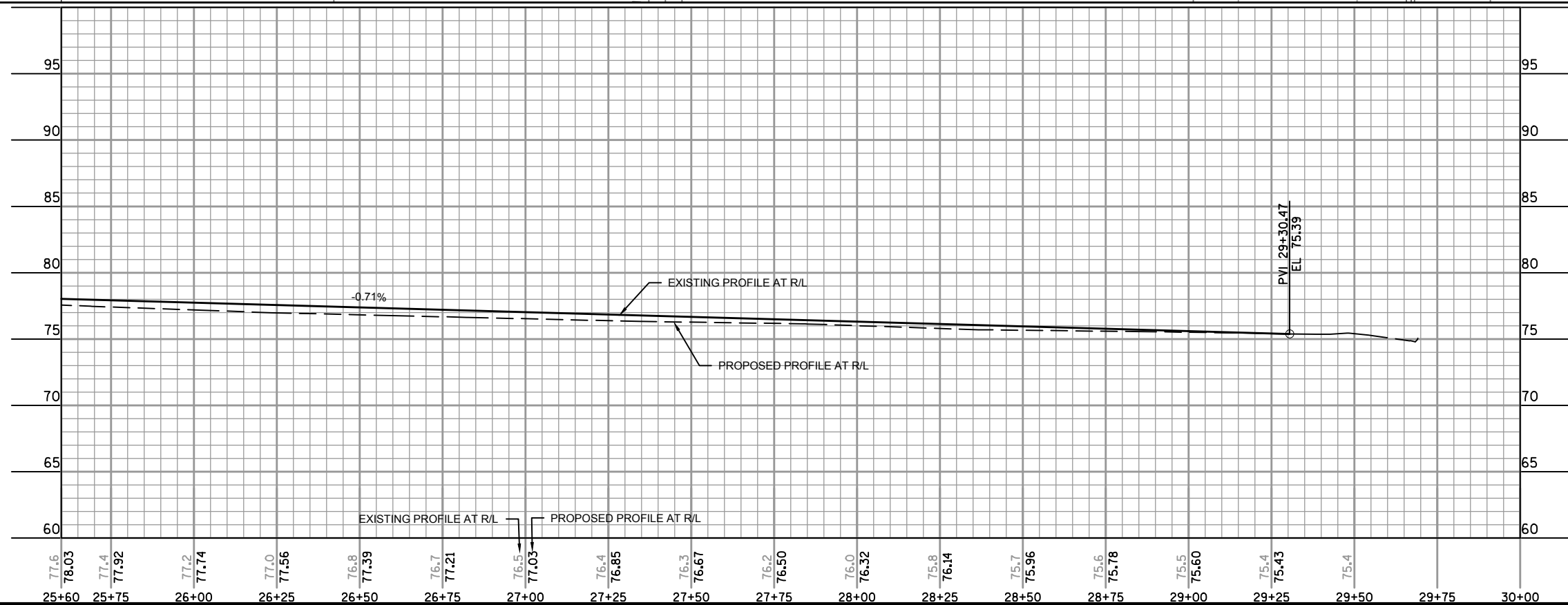
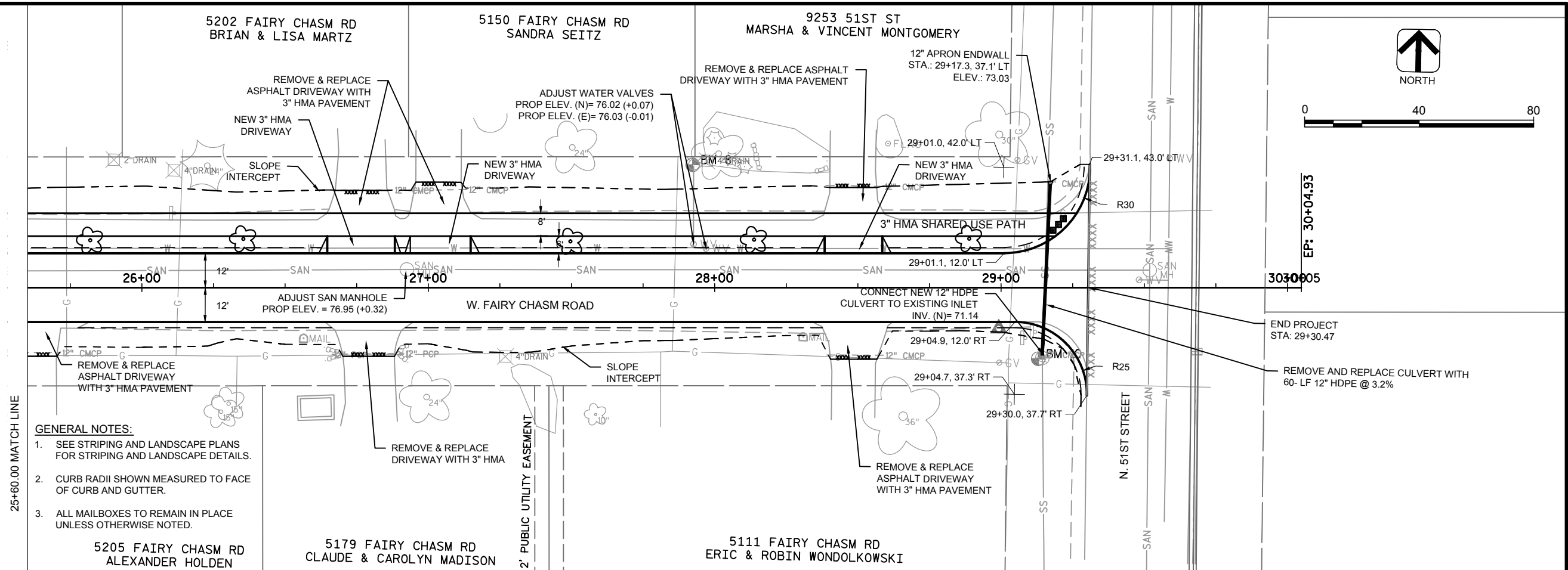
W. FAIRY CHASM ROAD RECONSTRUCTION
N. 60TH STREET TO N. 51ST STREET
VILLAGE OF BROWN DEER



PLAN AND PROFILE STA 19+45 - 25+60

SHEET NO
PP-4

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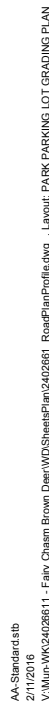
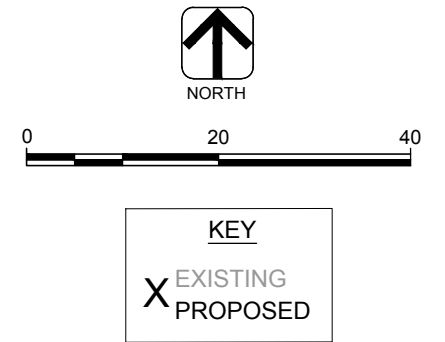
W. FAIRY CHASM ROAD RECONSTRUCTION
N. 60TH STREET TO N. 51ST STREET
VILLAGE OF BROWN DEER

AVRES
ASSOCIATES
WAUKESHA, WI

PLAN AND PROFILE STA 25+60 - 29+30.47

SHEET NO
PP-5

1. SEE STRIPING AND LANDSCAPE PLANS FOR STRIPING AND LANDSCAPE DETAILS.
2. CURB RADII SHOWN MEASURED TO FACE OF CURB AND GUTTER.
3. ALL MAILBOXES TO REMAIN IN PLACE UNLESS OTHERWISE NOTED.

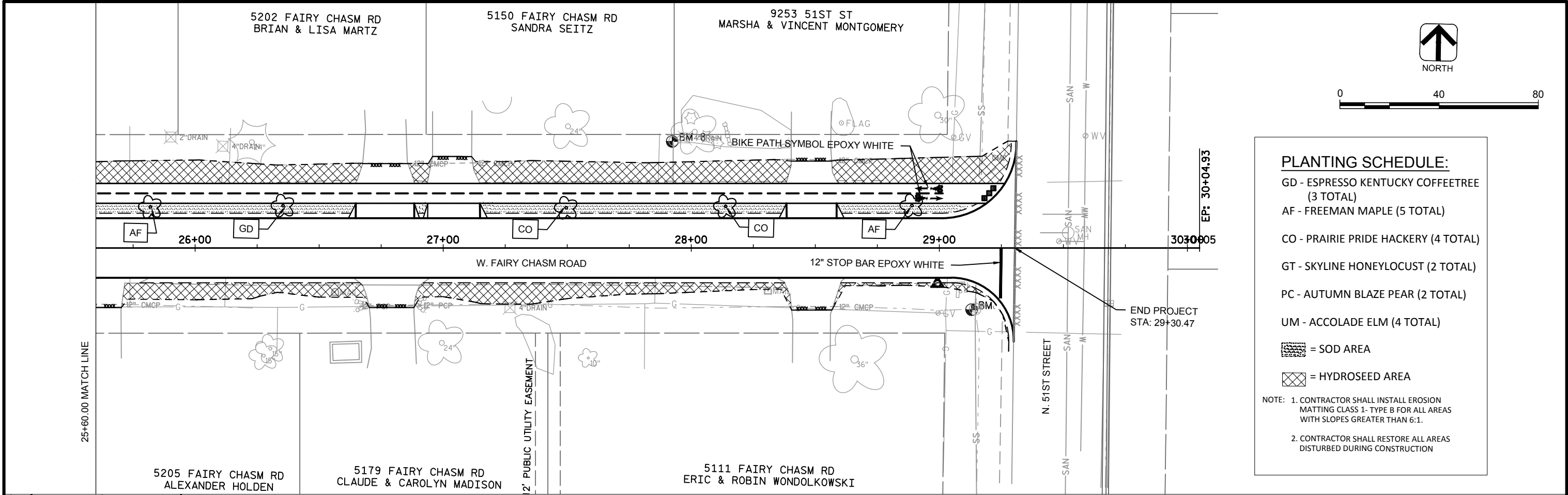


W. FAIRY CHASM ROAD RECONSTRUCTION
N. 60TH STREET TO N. 51ST STREET
VILLAGE OF BROWN DEER

PARK PARKING LOT GRADING PLAN

HEET NO

PP-6

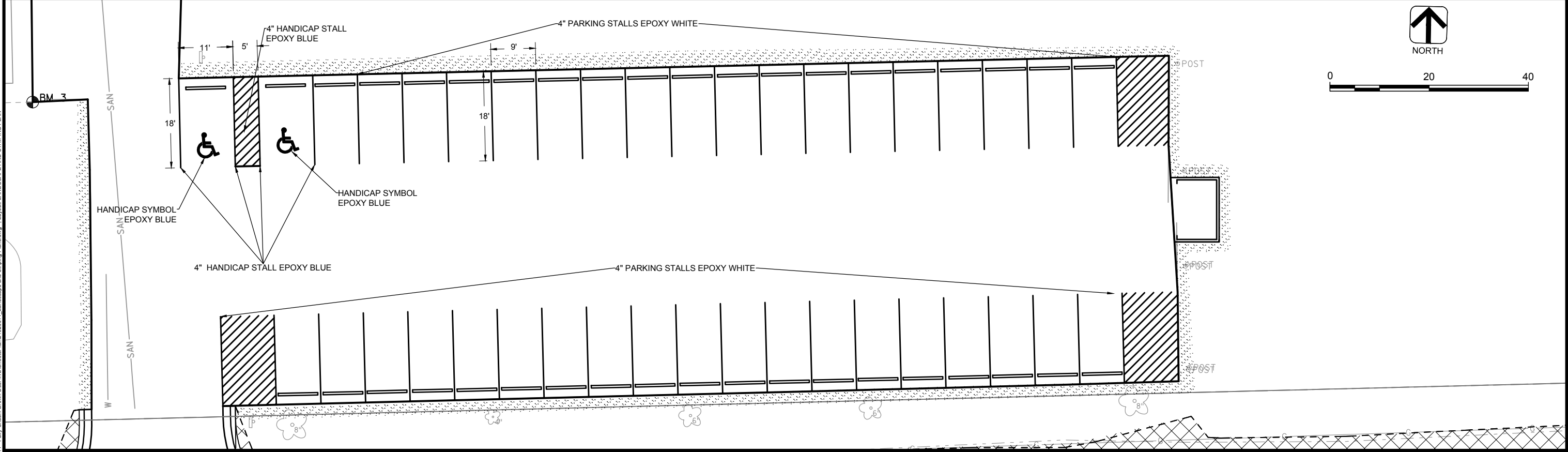


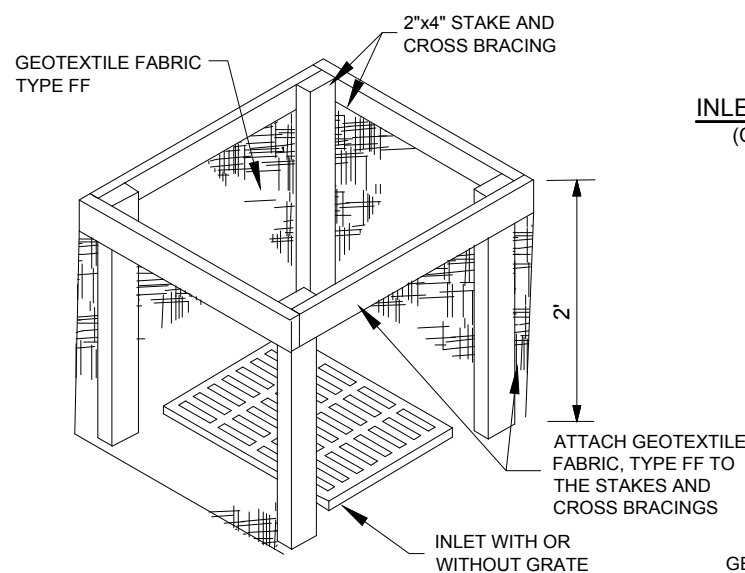
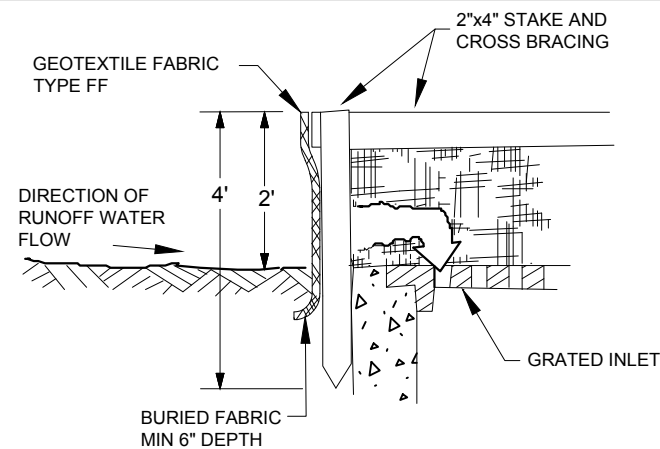
PLANTING SCHEDULE:

GD - ESPRESSO KENTUCKY COFFEETREE (3 TOTAL)
AF - FREEMAN MAPLE (5 TOTAL)
CO - PRAIRIE PRIDE HACKERY (4 TOTAL)
GT - SKYLINE HONEYLOCUST (2 TOTAL)
PC - AUTUMN BLAZE PEAR (2 TOTAL)
UM - ACCOLADE ELM (4 TOTAL)

= SOD AREA
 = HYDROSEED AREA

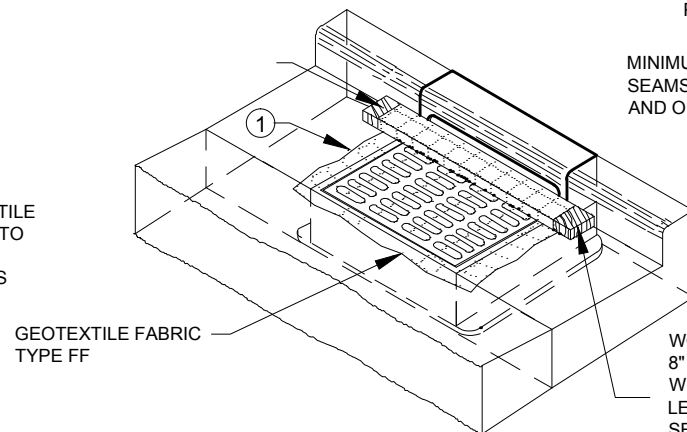
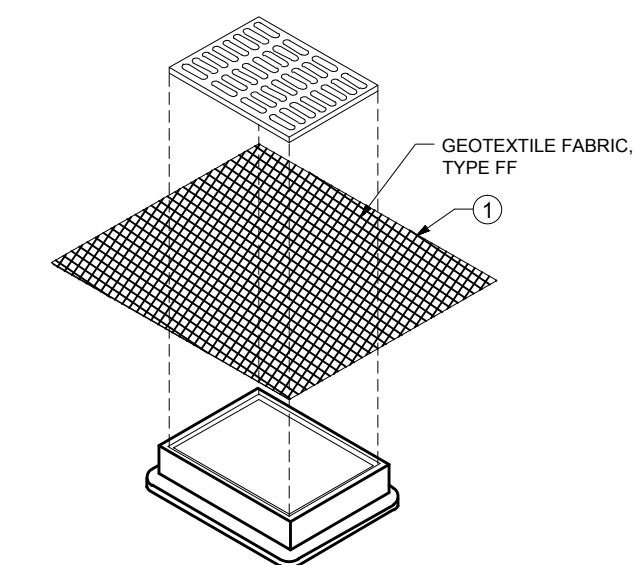
NOTE: 1. CONTRACTOR SHALL INSTALL EROSION MATTING CLASS 1- TYPE B FOR ALL AREAS WITH SLOPES GREATER THAN 6:1.
2. CONTRACTOR SHALL RESTORE ALL AREAS DISTURBED DURING CONSTRUCTION





INLET PROTECTION, TYPE A

INLET PROTECTION, TYPE B (WITHOUT CURB BOX)
(CAN BE INSTALLED ON ANY INLET WITHOUT CURB BOX)

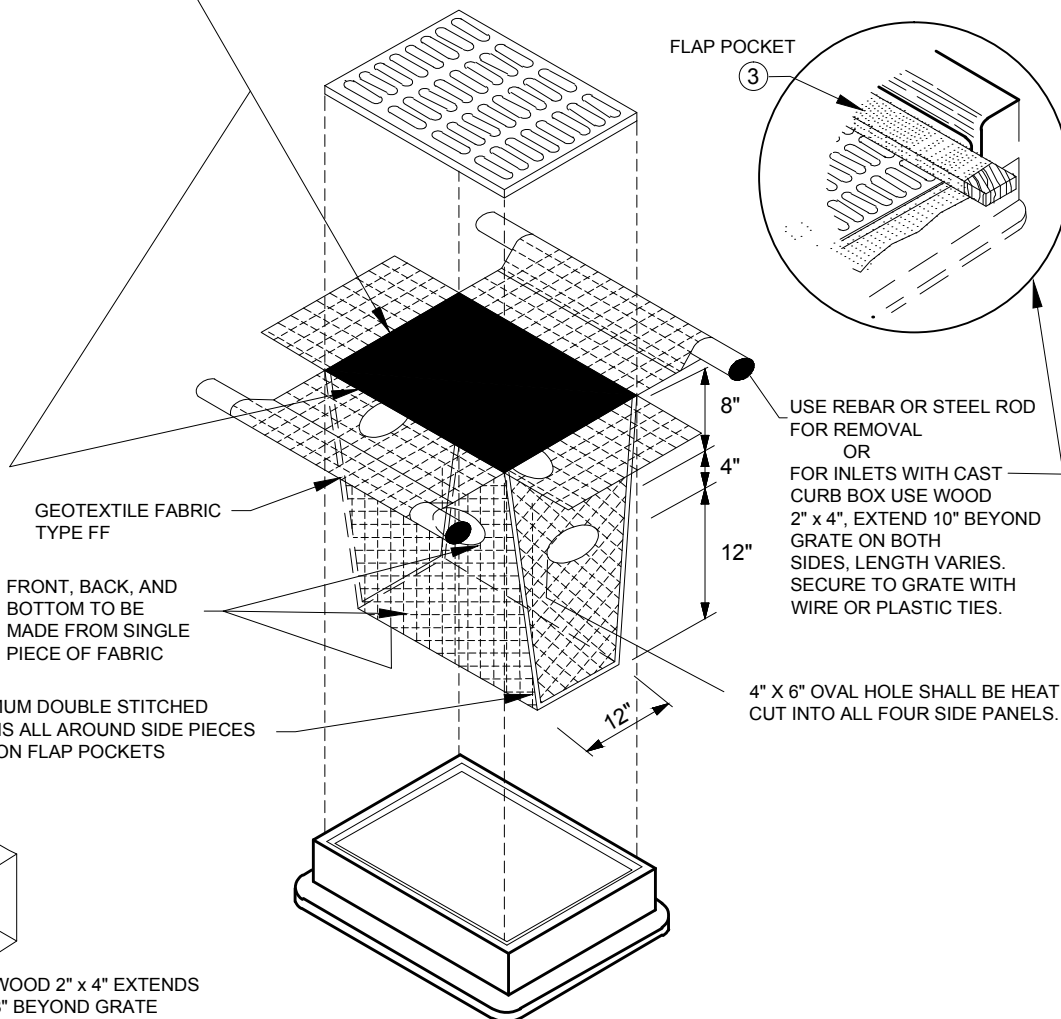


INLET PROTECTION, TYPE C (WITH CURB BOX)

WOOD 2" x 4" EXTENDS 8" BEYOND GRATE WIDTH ON BOTH SIDES. LENGTH VARIES. SECURE TO GRATE WITH WIRE OR PLASTIC TIES

FRONT, BACK, AND BOTTOM TO BE MADE FROM SINGLE PIECE OF FABRIC
MINIMUM DOUBLE STITCHED SEAMS ALL AROUND SIDE PIECES AND ON FLAP POCKETS

INLET SPECIFICATIONS AS PER THE PLAN
DIMENSION LENGTH AND WIDTH TO MATCH



INLET PROTECTION, TYPE D
(CAN BE INSTALLED ON ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

FLAP POCKET

③

USE REBAR OR STEEL ROD FOR REMOVAL OR FOR INLETS WITH CAST CURB BOX USE WOOD 2" x 4", EXTEND 10" BEYOND GRATE ON BOTH SIDES, LENGTH VARIES. SECURE TO GRATE WITH WIRE OR PLASTIC TIES.

4" X 6" OVAL HOLE SHALL BE HEAT CUT INTO ALL FOUR SIDE PANELS.

GENERAL NOTES

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION TYPE C WITH CURB BOX, AN ADDITIONAL 18" FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.

INSTALLATION NOTES

TYPES A, B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE. THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.

This drawing based on Wisconsin
Department of Transportation
Standard Detail Drawing 8 E 10-2.

**INLET PROTECTION
TYPE A, B, C, AND D**



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W. FAIRY CHASM ROAD RECONSTRUCTION
N. 60TH STREET TO N. 51ST STREET
VILLAGE OF BROWN DEER



CONSTRUCTION DETAILS

SHEET NO
DT-1

GENERAL NOTES

- 1
- TRENCH SHALL BE A MIN OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC, FOLD THE MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.

- 2
- WOOD POSTS SHALL BE A MIN SIZE OF 1 1/8" x 1 1/8" OAK OR HICKORY.

- 3
- CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY, USE ONE OF THE FOLLOWING TWO METHODS:
A. TWIST METHOD -- OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES.
B. HOOK METHOD -- HOOK THE END OF EACH SILT FENCE LENGTH.

NOTE:
ADDITIONAL POST DEPTH OR TIE BACKS
MAY BE REQUIRED IN UNSTABLE SOLS

WOOD POSTS
LENGTH 3'-4"
20" DEPTH
IN GROUND

GEOTEXTILE
FABRIC ONLY

BACKFILL & COMPACT
TRENCH WITH
EXCAVATED SOIL

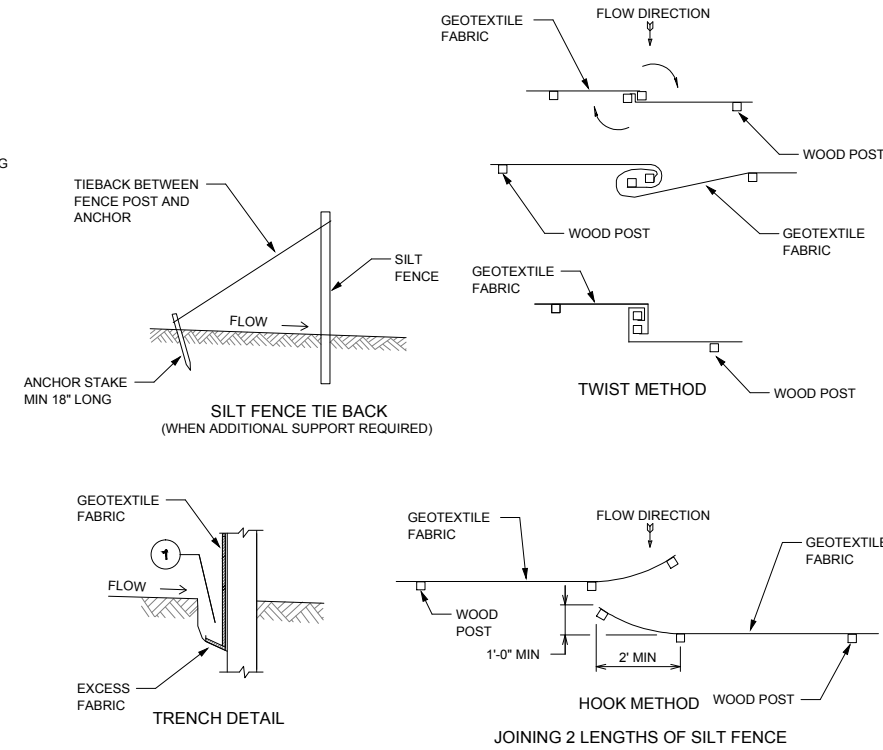
ATTACH FABRIC TO
TIE POSTS WITH WIRE
STAPLES OR WOODEN
LATH AND NAILS

*NOTE:
8'-0" POST SPACING ALLOWED IF A
WOVEN GEOTEXTILE FABRIC IS USED

SILT FENCE

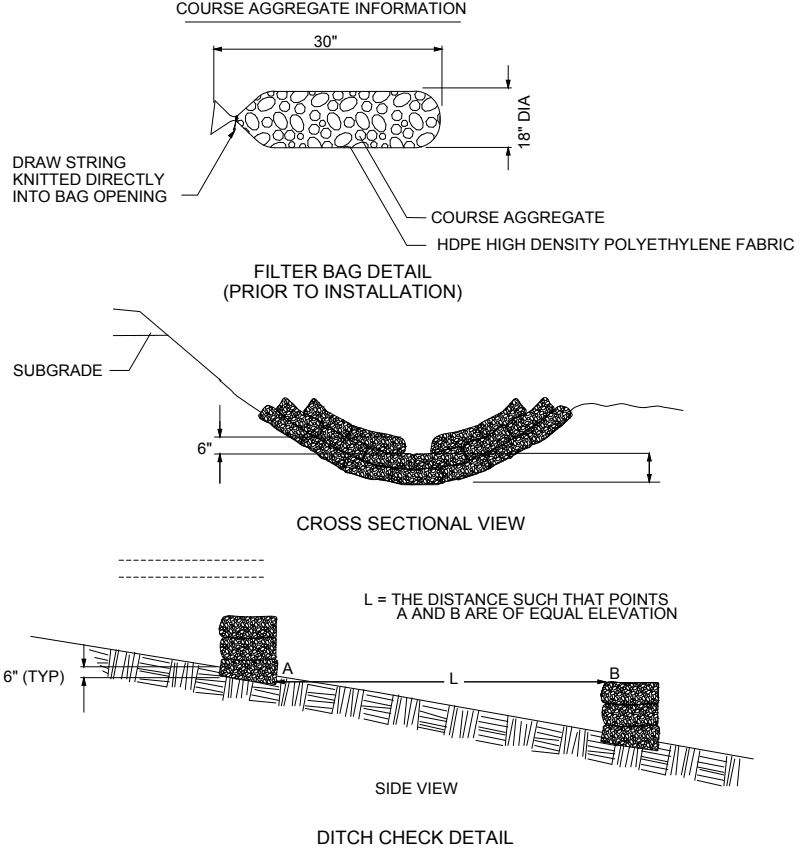
SP
100

SILT FENCE
NOT TO SCALE



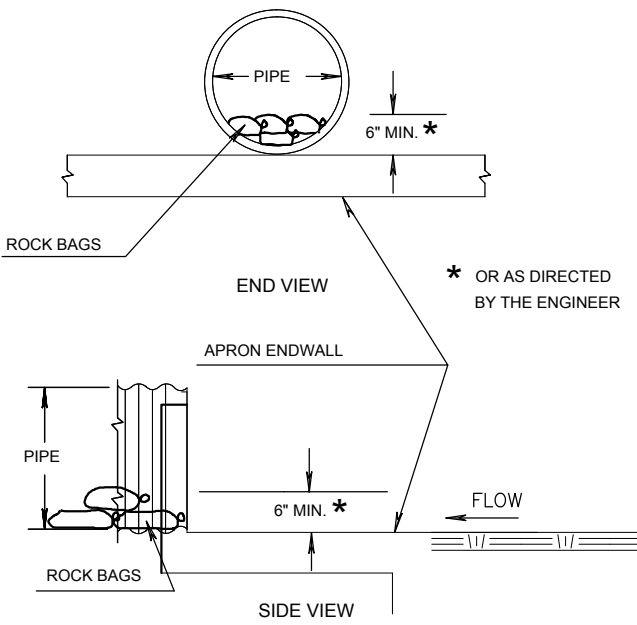
GENERAL NOTES:
18" X 30" ROCK FILLED FILTER BAG SHALL
BE COMPRISED OF THE FOLLOWING:
-HDPE HIGH DENSITY POLYETHYLENE
-HDPE HIGH DENSITY POLYETHYLENE DRAW
STRING KNITTED DIRECTLY INTO BAG
OPENING.
-80% FABRIC CLOSURE WITH APPARENT
OPENING SIZE NO LARGER THAN "X"
-ROLLED SEAM USING A MINIMUM OF 480
DENIER POLYESTER SEWING YARN FOR
STRENGTH AND DURABILITY.
USE WELL GRADED COARSE AGGREGATE CONFORMING TO THE
FOLLOWING GRADATION REQUIREMENTS

SIEVE SIZE	SIZE NO.
2 INCH (50 mm)	AASHTO No. 67
1.5 INCH (37.5mm)	
1 INCH (25.0 mm)	100
0.75 INCH (19.0mm)	90-100
0.5 INCH (9.5mm)	20-55
No. 4 (4.75mm)	0-10
No. 8 (2.36mm)	0-5
(1) SIZE No. ACCORDING TO AASHTO M 43	



DTL
201

ROCK BAG DITCH CHECK
NTS



DTL
202

CULVERT PIPE ROCK BAG DITCH CHECK
NTS

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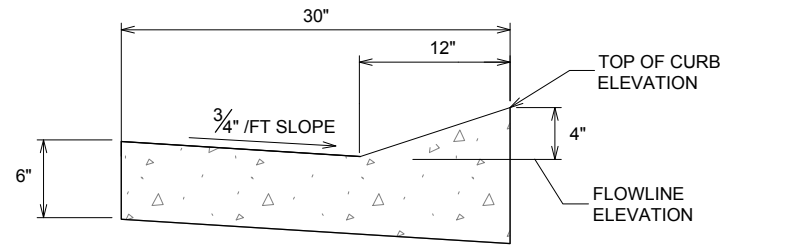
W. FAIRY CHASM ROAD RECONSTRUCTION
N. 60TH STREET TO N. 51ST STREET
VILLAGE OF BROWN DEER



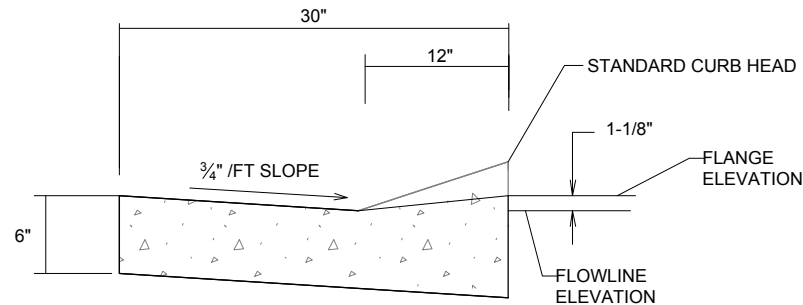
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SHEET NO
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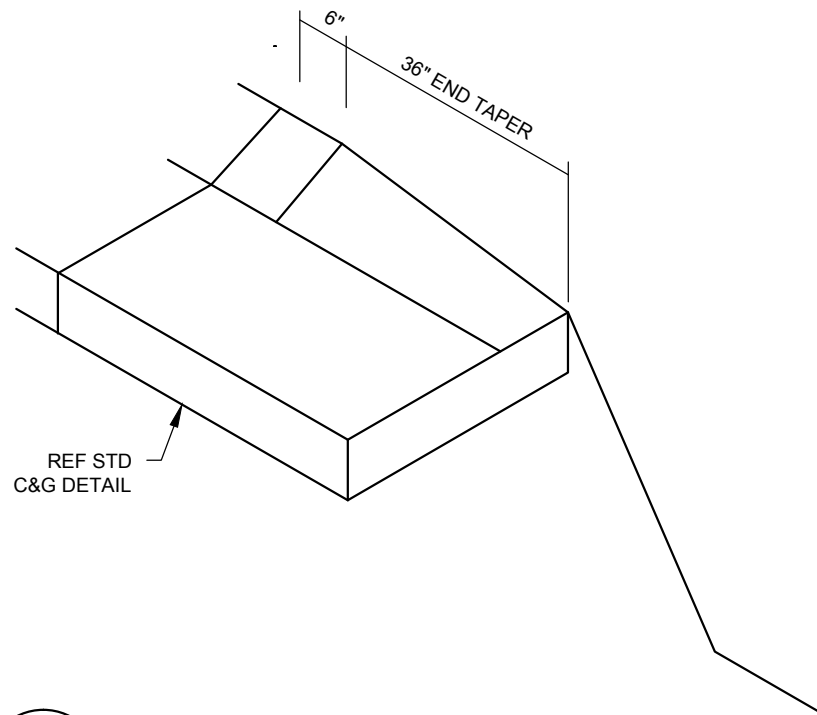


STANDARD CURB

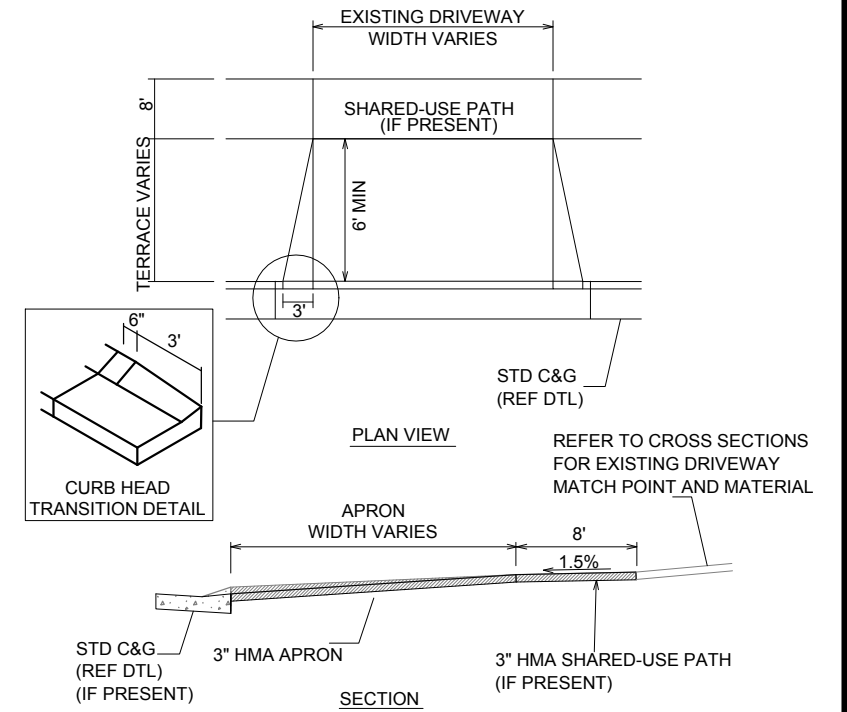


DRIVEWAY CURB

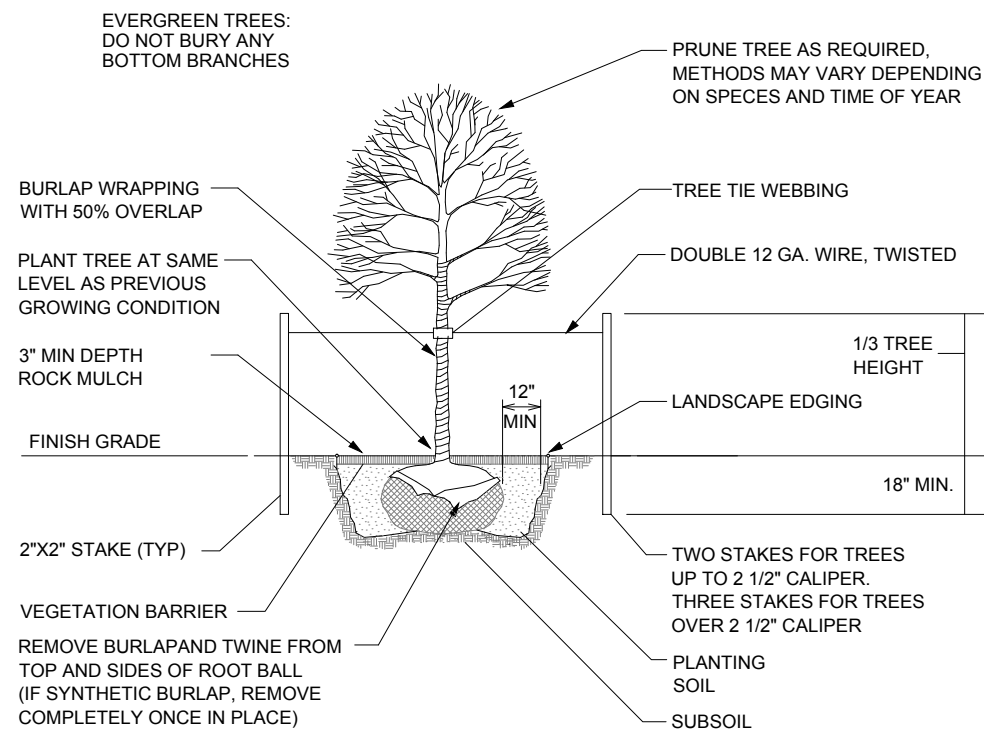
DTL 300 **30" "ROLL TYPE" CURB AND GUTTER**
NTS



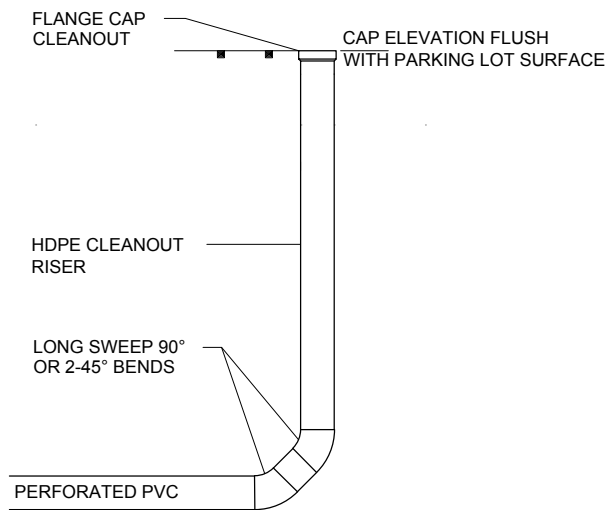
DTL 301 **CURB AND GUTTER TERMINATOR**
NTS



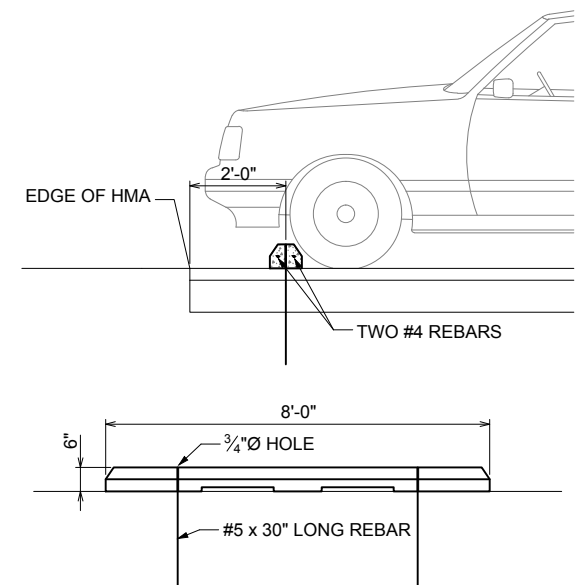
DTL 302 **ASPHALT DRIVEWAY APRON**
NTS



DTL 303 **TREE PLANTING DETAIL**
NTS



DTL 304 **UNDERDRAIN CLEANOUT**
NTS



DTL 305 **PRECAST CONC WHEEL STOP**
NTS

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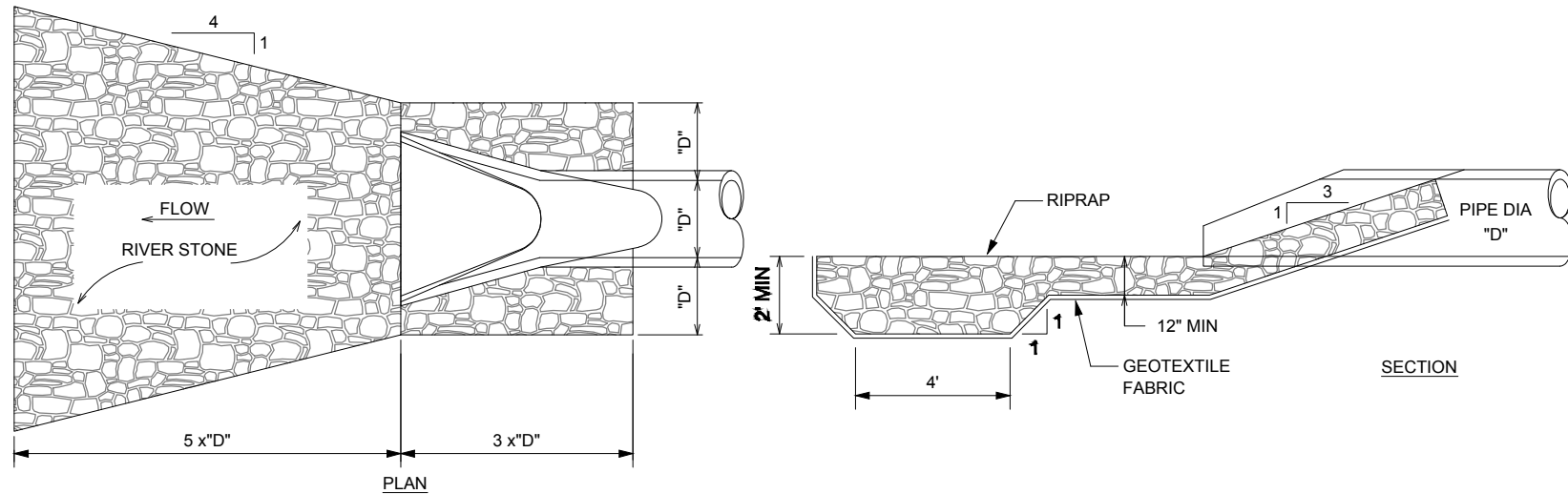
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N. 60TH STREET TO N. 51ST STREET
VILLAGE OF BROWN DEER

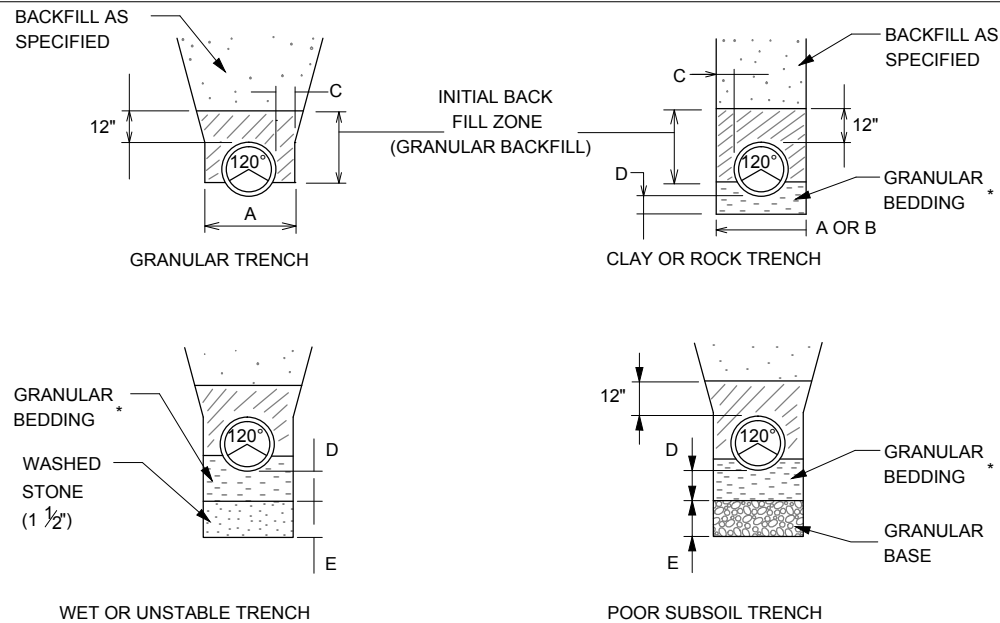
AYRES
ASSOCIATES
WAUKESHA, WI

CONSTRUCTION DETAILS

SHEET NO
DT-3



DTL 400 RIVER STONE - STORM SEWER ENDWALL
NTS



A=O.D. PIPE + 24"(MAX)
B=O.D. PIPE + 18"(MAX) FOR ROCK
C=6" MIN.
D=4" BELOW BARREL FOR CLAY
3" BELOW BELL FOR CLAY
6" BELOW BARREL FOR ROCK
E=DETERMINED BY A/E

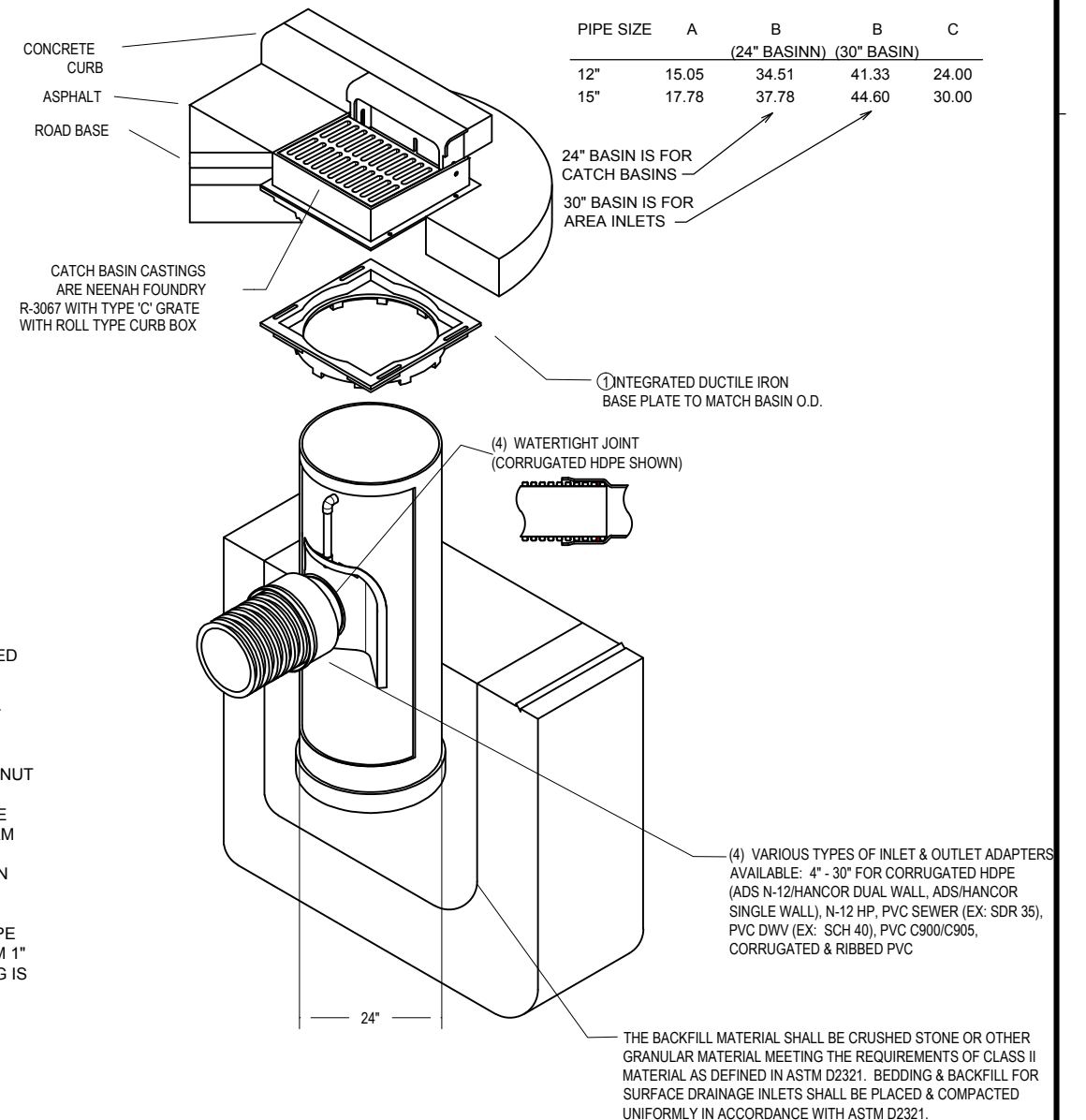
* FOR FLEXIBLE PIPE,
BEDDING MATERIAL SHALL
EXTEND TO SPRING LINE
(MID HEIGHT) OF PIPE.

DTL 401 TRENCH SECTIONS
NTS

- MEASURE 3.50" UP FROM THE TOP OF THE OUTLET PIPE INSIDE DIAMETER & MAKE A HORIZONTAL MARK
- INSERT THE SNOOT & PLACE THE TOP EDGE OF THE FLANGE ON THE HORIZONTAL MARK. POSITION THE SNOOT DIRECTLY OVER THE OUTLET PIPE SO THAT THE ENTIRE OUTLET IS COVERED & THE FLANGES OF THE SNOOT DO NOT BLOCK THE INLET PIPES (SEE DRAWING NO. 7004-110-043, 7004-110-044, & 7004-110-045 FOR MINIMUM ANGLE BETWEEN ADAPTERS).
- MARK & PRE-DRILL (TO MATCH THE HOLES IN THE SNOOT) 3/16" PILOT HOLES. FOR THE #14 X 1 1/4" SS SELF TAPPING HEX DRIVE SCREWS, THAT ARE PROVIDED IN THE INSTALL KIT. INSTALL 1#4 X 1" NEOPRENE BACKED WASHERS ON EACH SCREW.
- ATTACH THE 1" VENT PIPE ADAPTER IN THE PRE-DRILLED HOLE ON THE TOP OF THE SNOOT, USING THE 2 FLAT O RING GASKETS & PVC LOCK-NUT THAT ARE SUPPLIED IN THE KIT. INSTALL THE ADAPTER, WITH THE FEMALE SLIP ADAPTER UP & A WASHER ON EACH SIDE OF THE SNOOT SHELL. TIGHTEN THE PVC LOCK-NUT BY HAND.
- REMOVE THE PSA BACKING FROM THE GASKET STRIP THAT IS PROVIDED IN THE KIT. ATTACH THE GASKET STRIP TO THE BACK OF THE SNOOT FLANGE WITH FIRM PRESSURE, & TRIM ANY EXCESS GASKET MATERIAL.
- ATTACH THE SNOOT TO THE DRAIN BASIN WITH THE #14 SS SCREWS, & TIGHTEN TO APPROXIMATELY 10-15 LBS.
- CUT THE ANTI-SIPHON AIR VENT PIPE TO LENGTH & ATTACH TO SNOOT AT SLIP ADAPTER WITH PVC CEMENT. ATTACH THE 90 DEGREE ELBOW TO THE VENT PIPE WITH PVC CEMENT. THE TOP OF THE 90 DEGREE ELBOW SHOULD BE A MINIMUM 1" FROM THE BOTTOM OF THE GRATE FRAME. ENSURE THAT THE ELBOW OPENING IS ACCESSIBLE FOR MAINTENANCE & INSPECTION.

Nyloplast
3130 VERONA AVE
BUFORD, GA 30518
PHN (770) 932-2443
FAX (770) 932-2490
www.nyloplast-us.com

DTL 402 NYLOPLAST CATCH BASIN AND AREA INLET WITH SUMP AND SNOOT
NTS



PIPE SIZE	A	B (24" BASINN)	B (30" BASIN)	C
12"	15.05	34.51	41.33	24.00
15"	17.78	37.78	44.60	30.00

24" BASIN IS FOR
CATCH BASINS
30" BASIN IS FOR
AREA INLETS

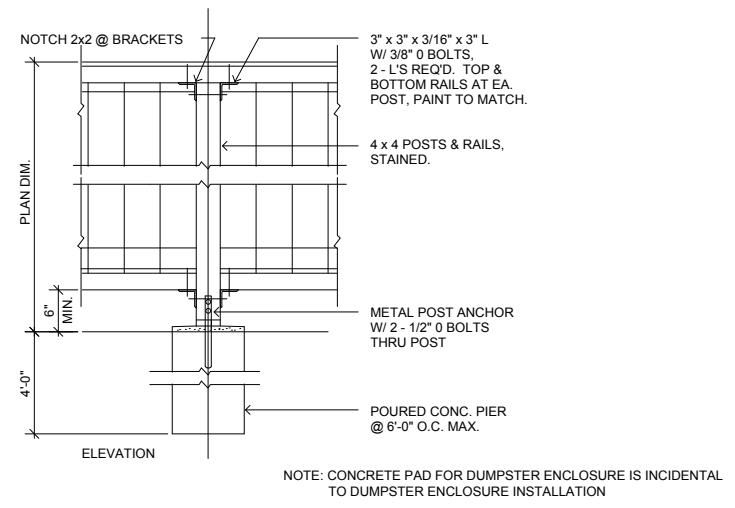
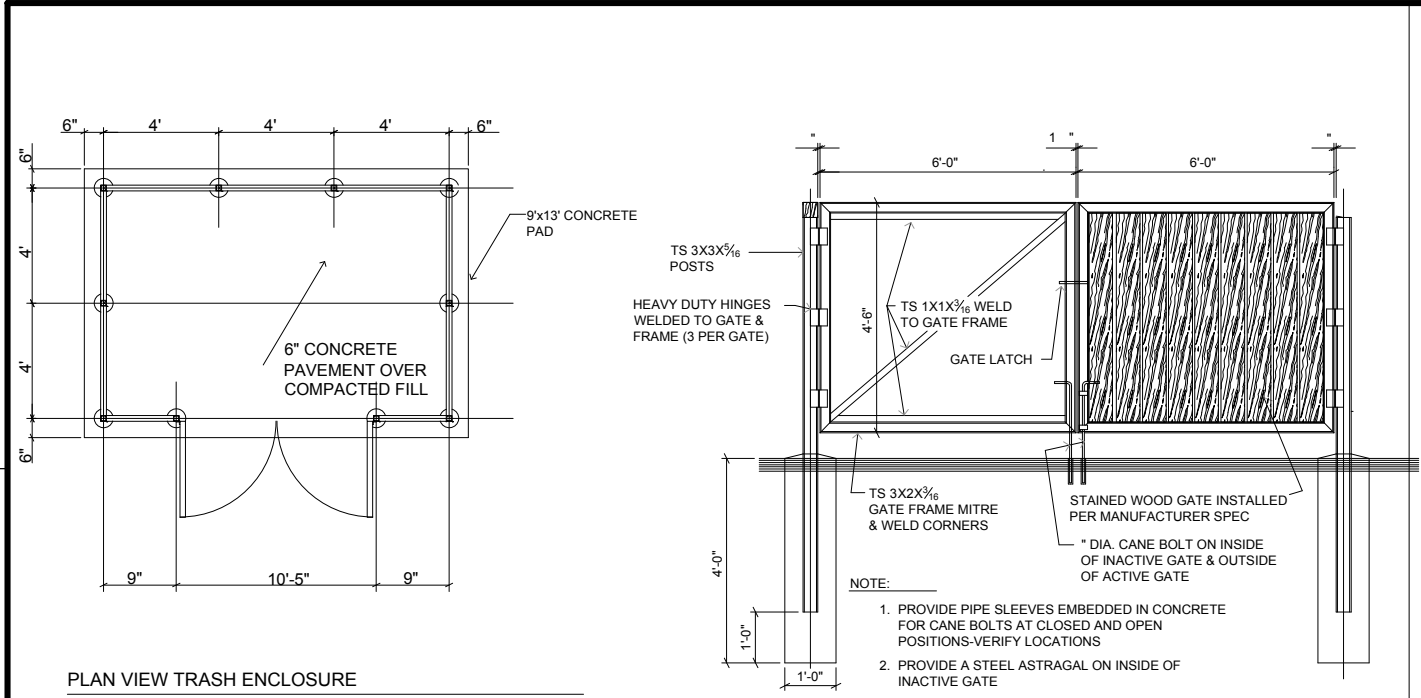
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N. 60TH STREET TO N. 51ST STREET
VILLAGE OF BROWN DEER

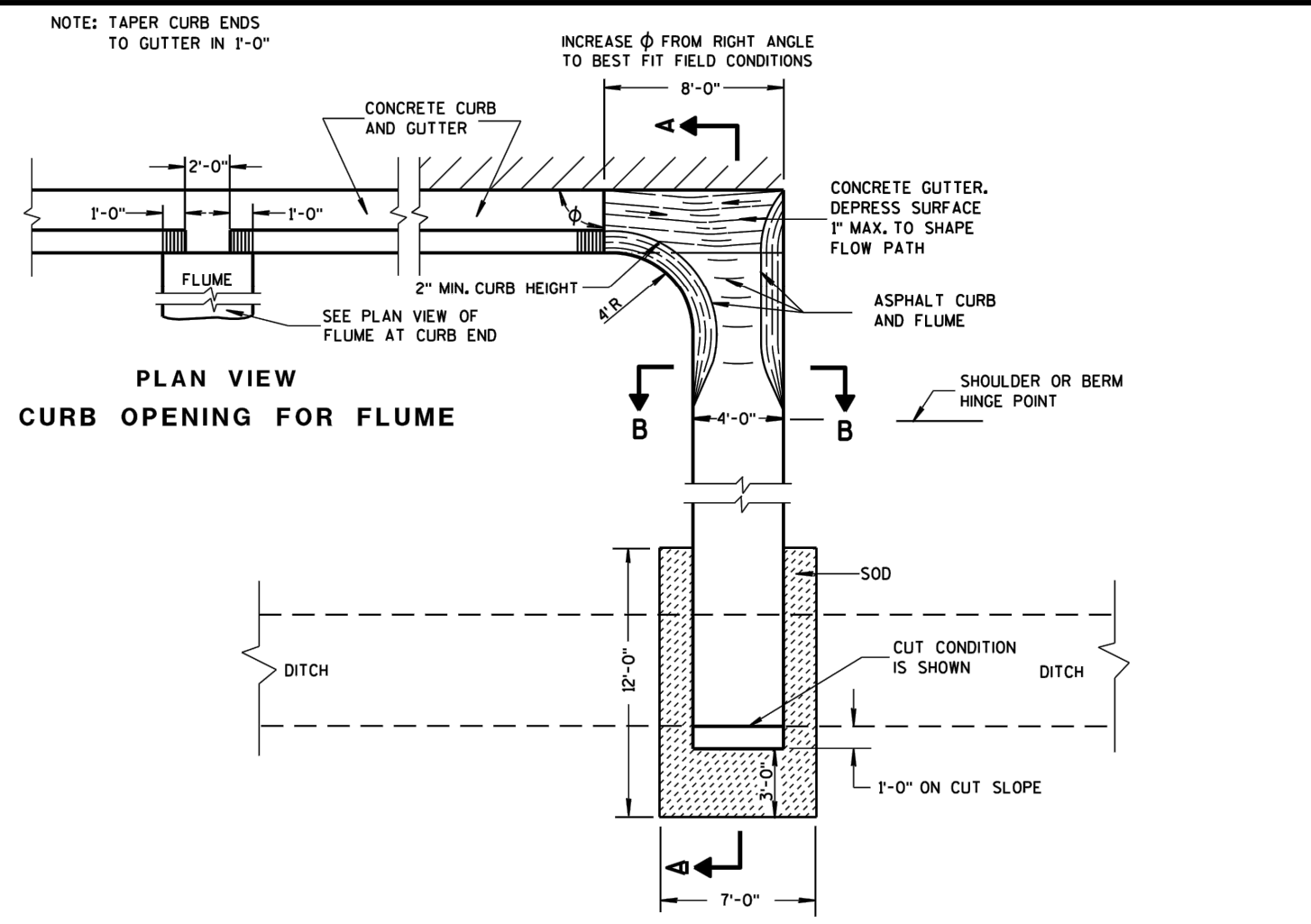
AYRES
ASSOCIATES
WAUKESHA, WI

CONSTRUCTION DETAILS

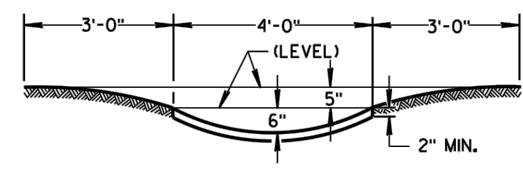
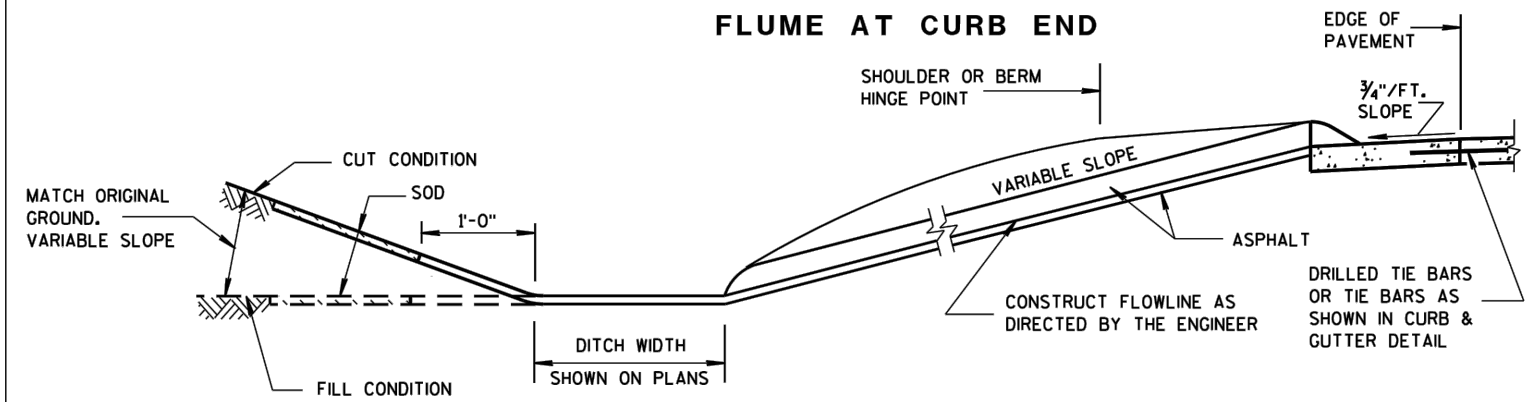
SHEET NO
DT-4



DTL 500 TRASH ENCLOSURE
NTS



PLAN VIEW FLUME AT CURB END



DTL 501 ASPHALT FLUME
NTS

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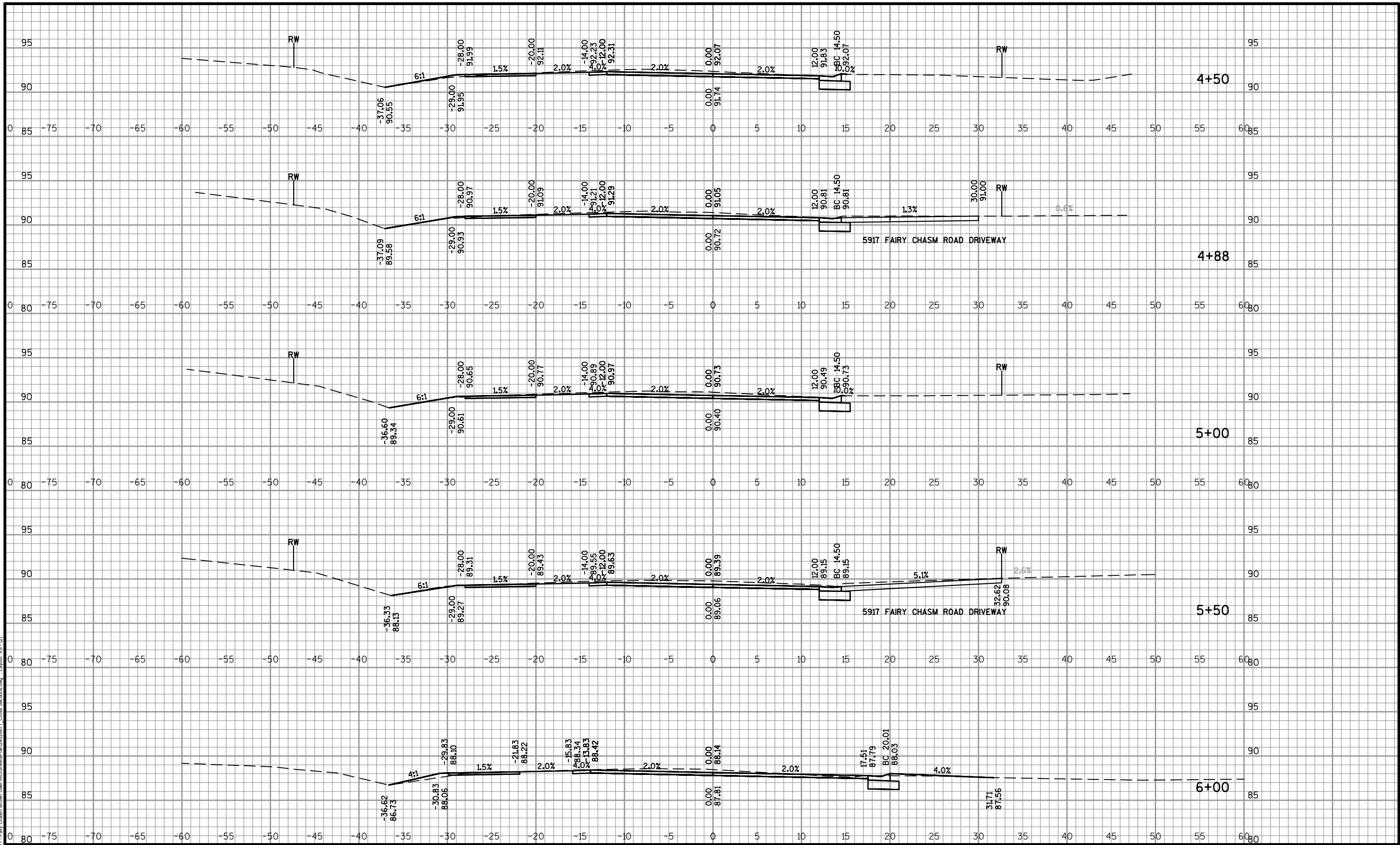
W. FAIRY CHASM ROAD RECONSTRUCTION
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VILLAGE OF BROWN DEER

AYRES ASSOCIATES
WAUKESHA, WI

CONSTRUCTION DETAILS

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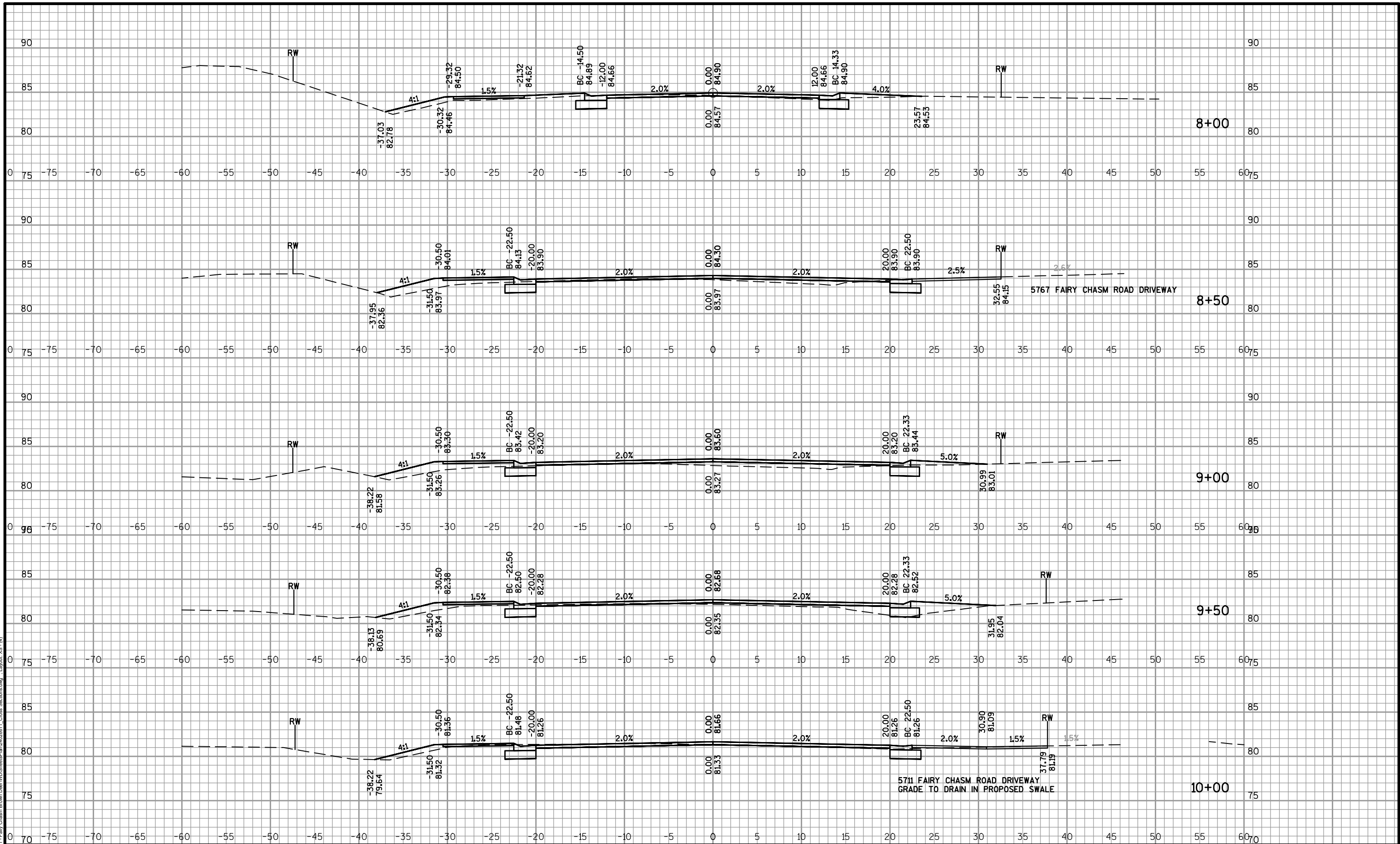
W. FAIRY CHASM ROAD RECONSTRUCTION
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CROSS SECTIONS

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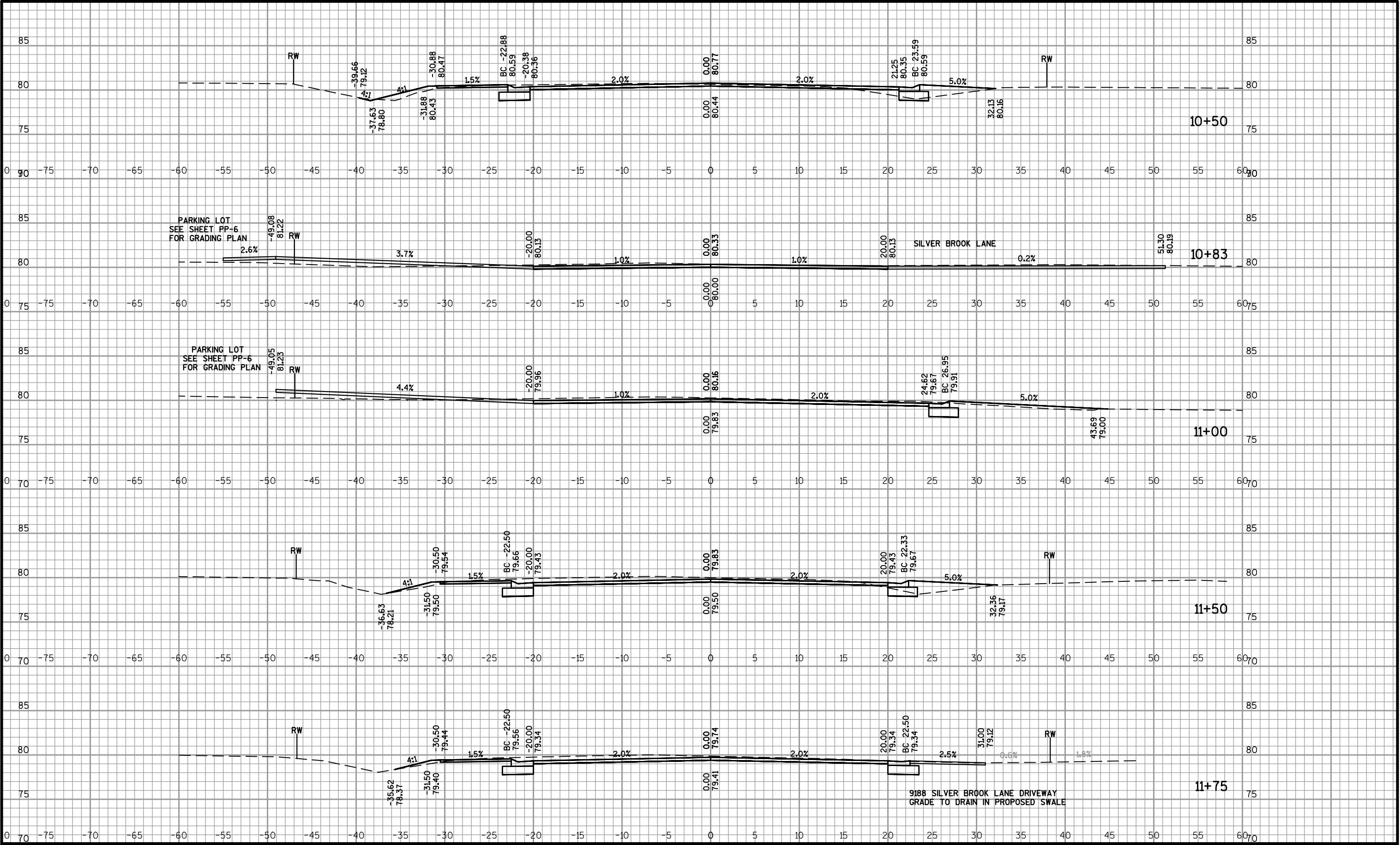
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VILLAGE OF BROWN DEER



CROSS SECTIONS

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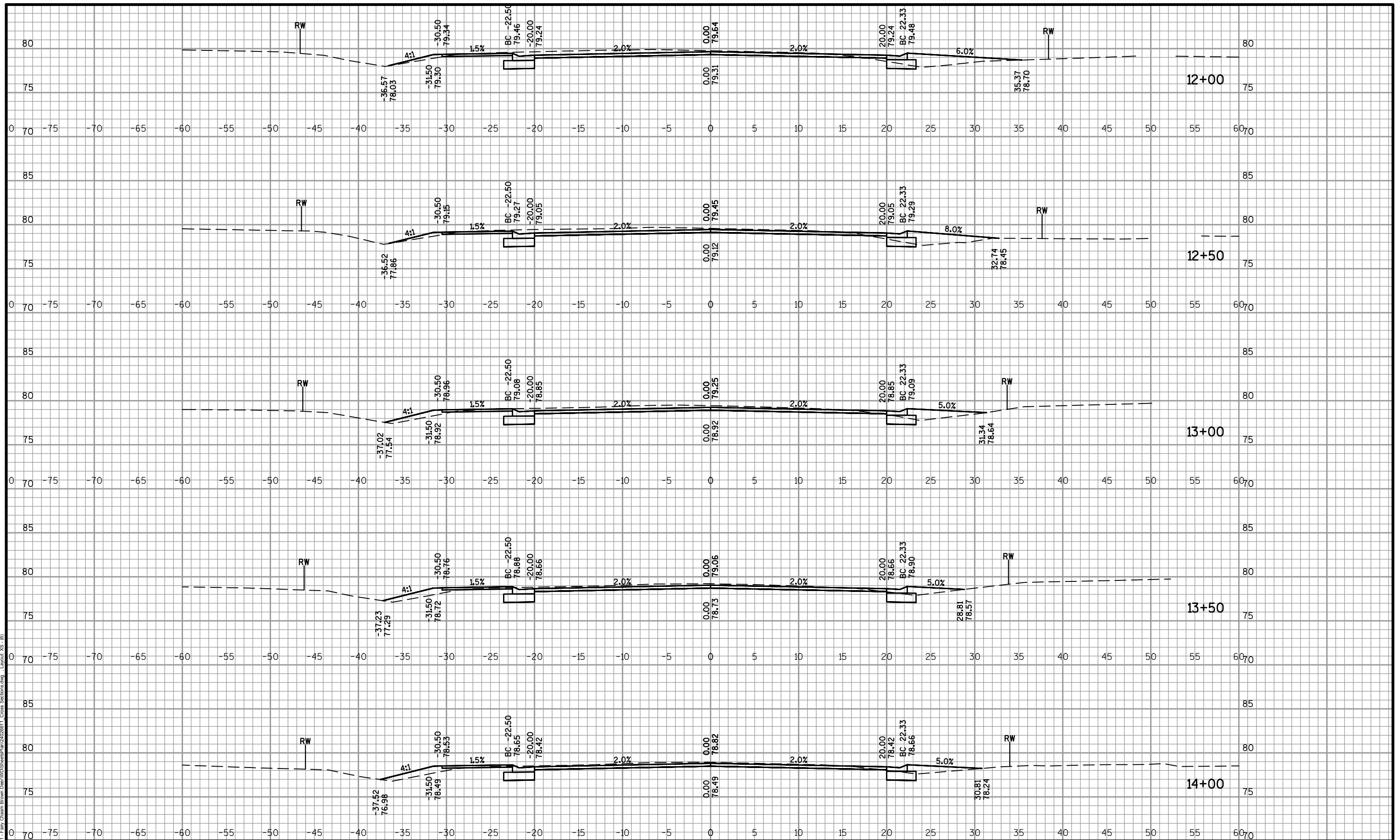
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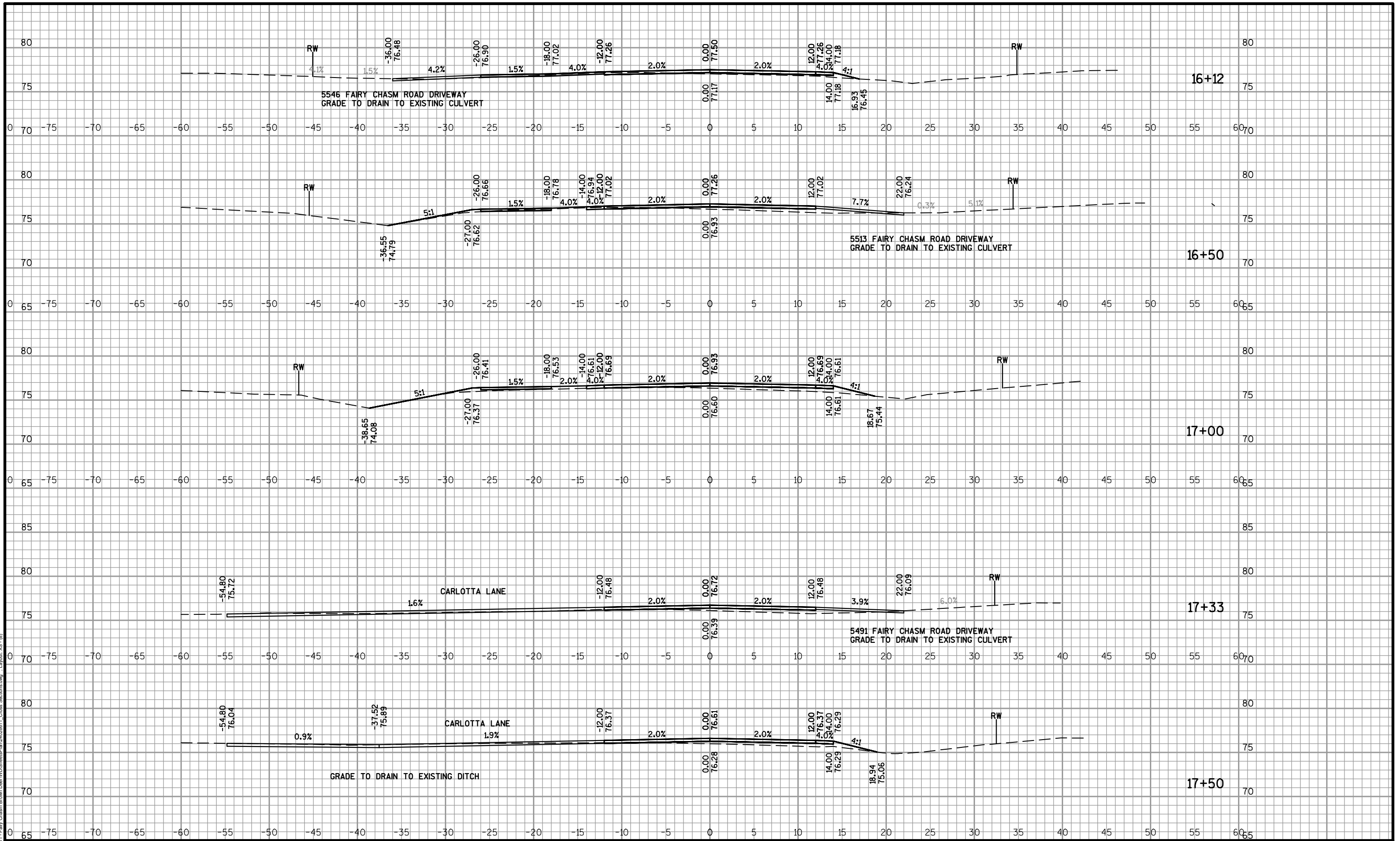
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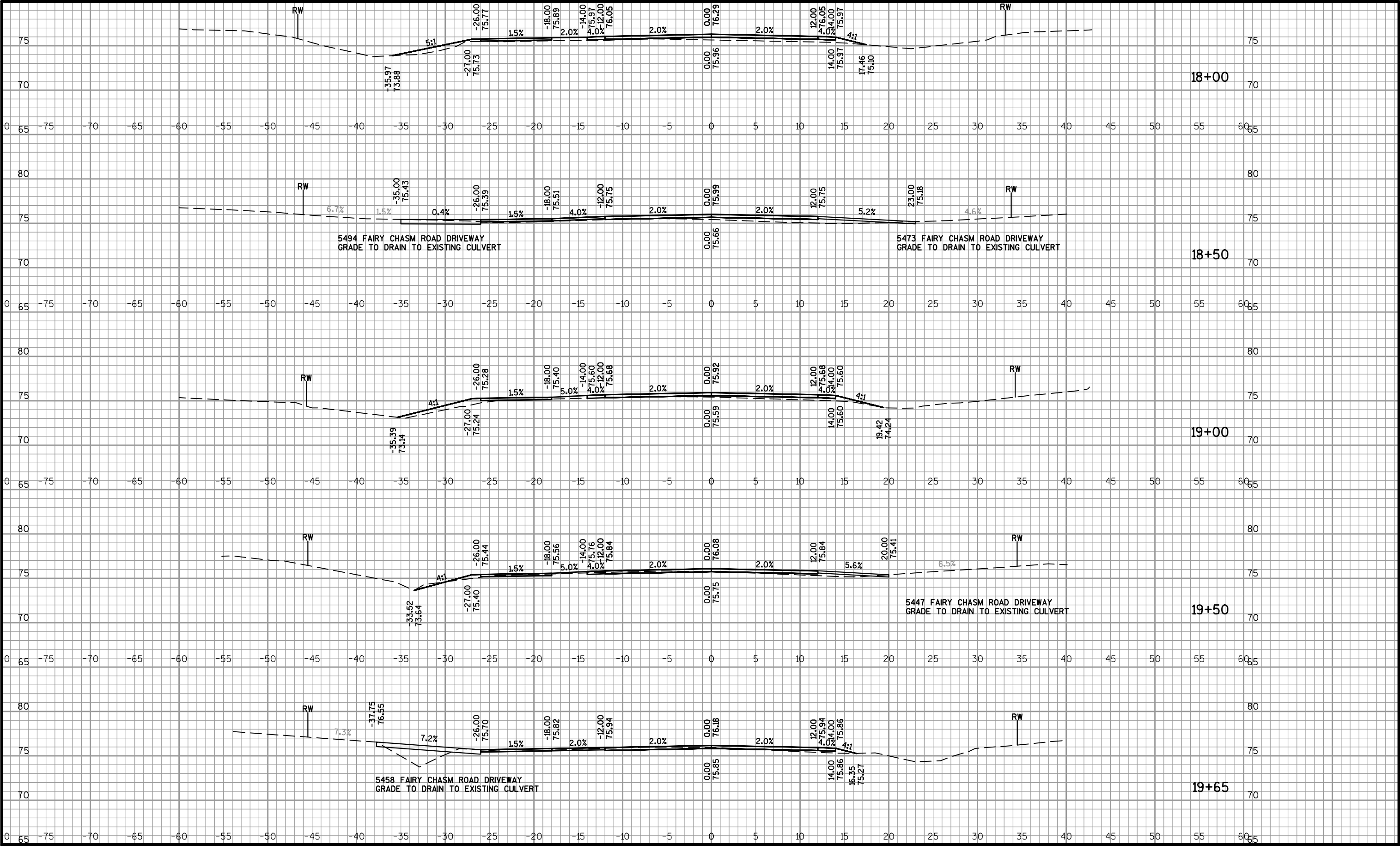
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VILLAGE OF BROWN DEER



CROSS SECTIONS

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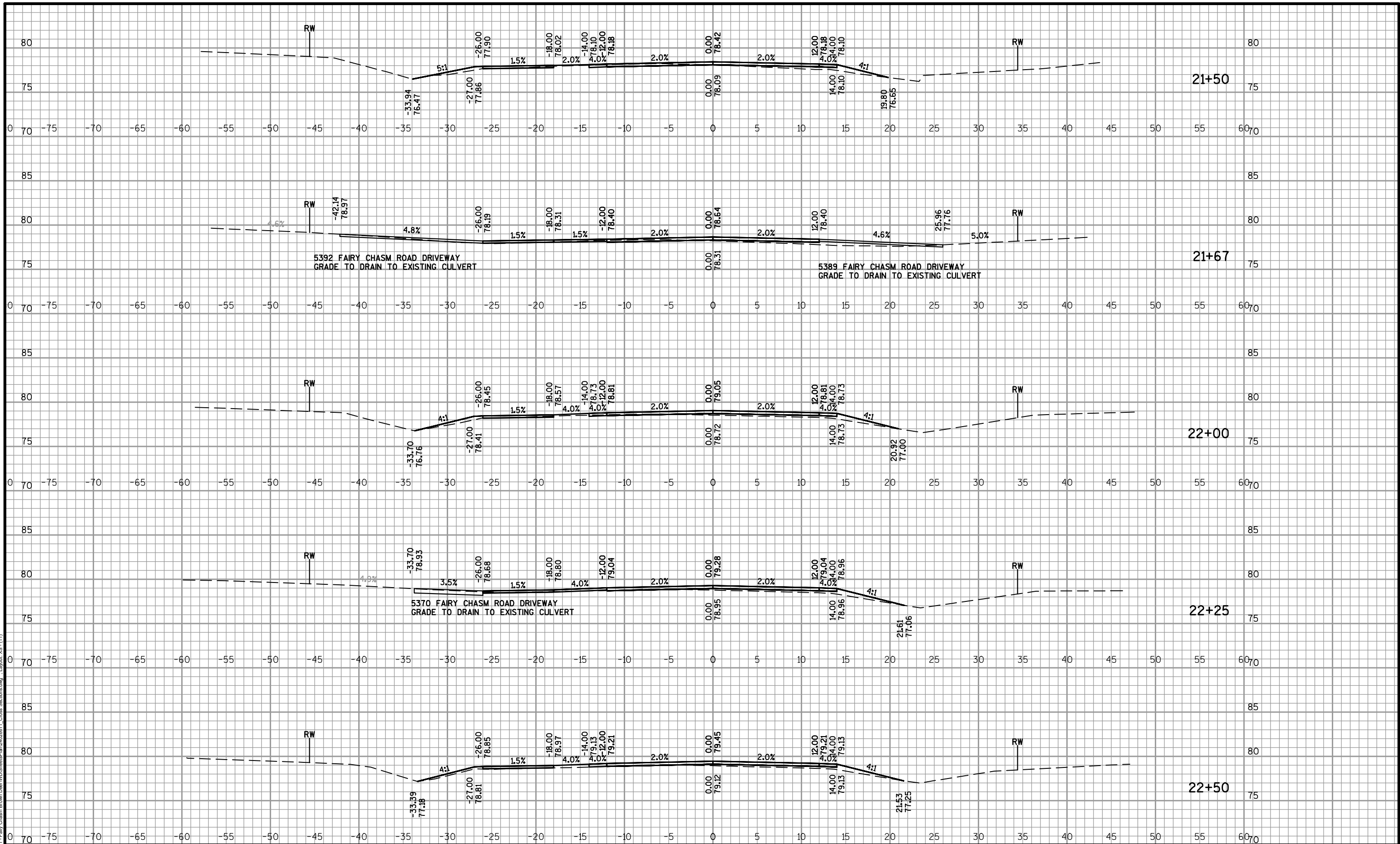
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CROSS SECTIONS

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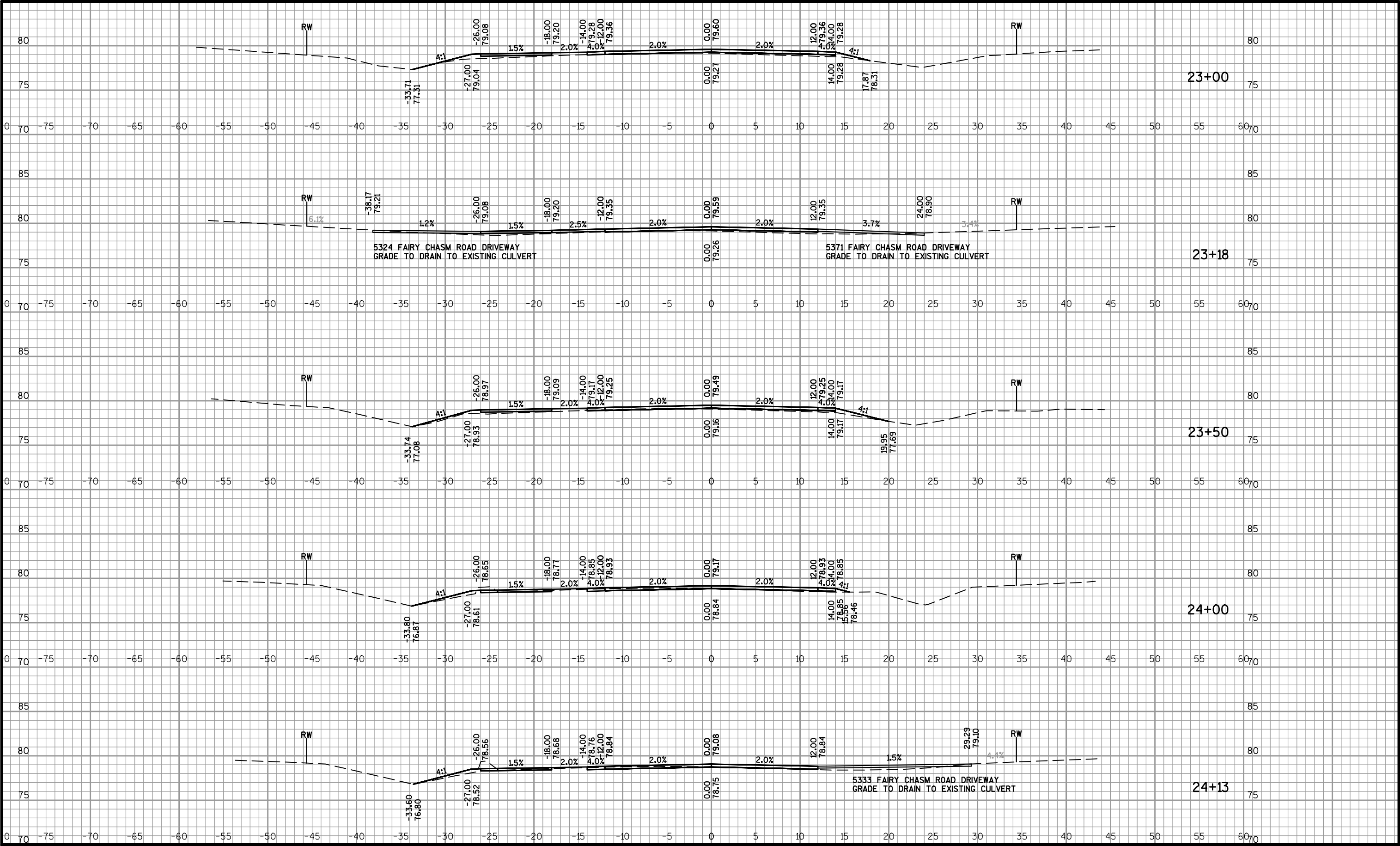
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N. 60TH STREET TO N. 51ST STREET
VILLAGE OF BROWN DEER



CROSS SECTIONS

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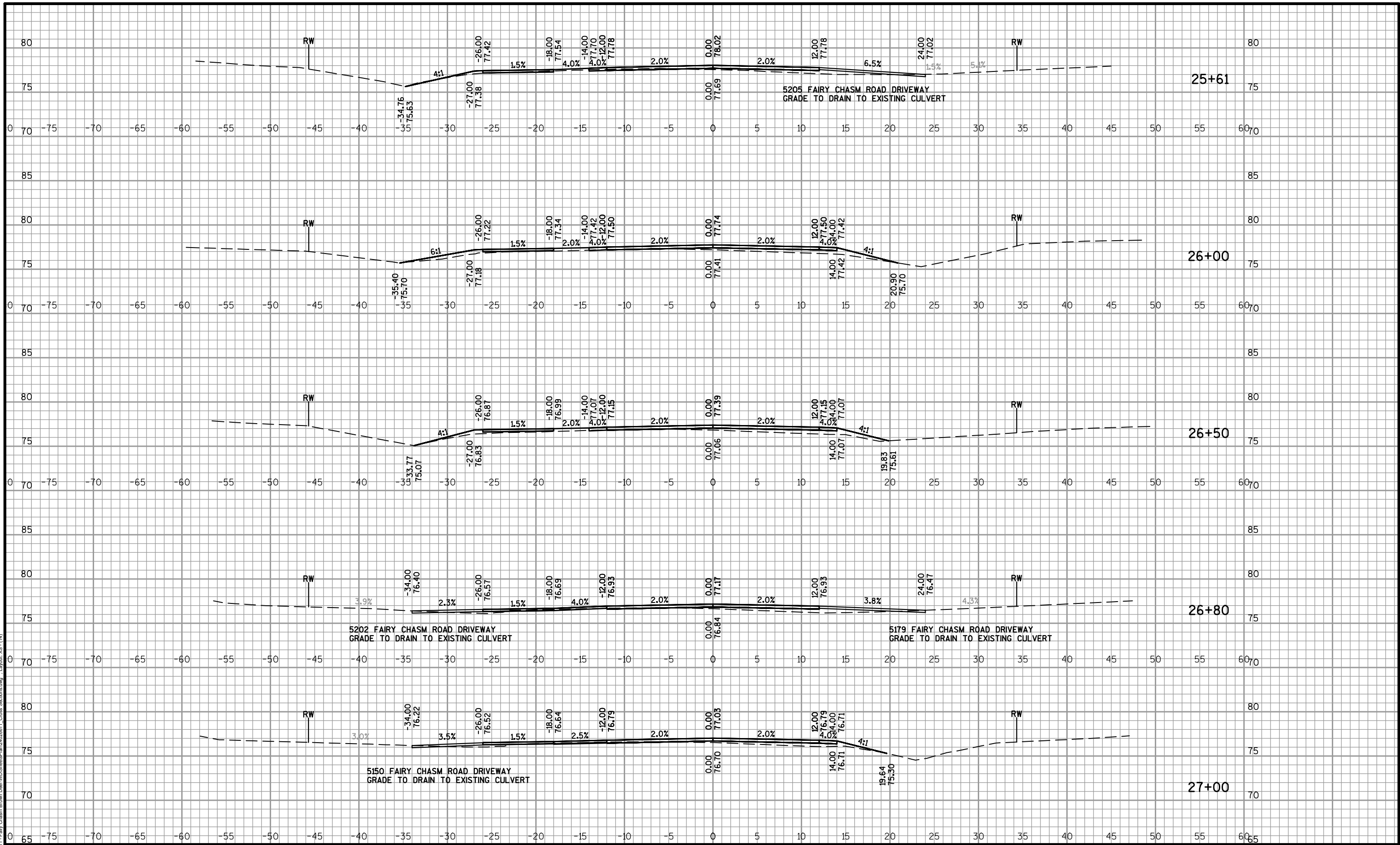
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N. 60TH STREET TO N. 51ST STREET
VILLAGE OF BROWN DEER



CROSS SECTIONS

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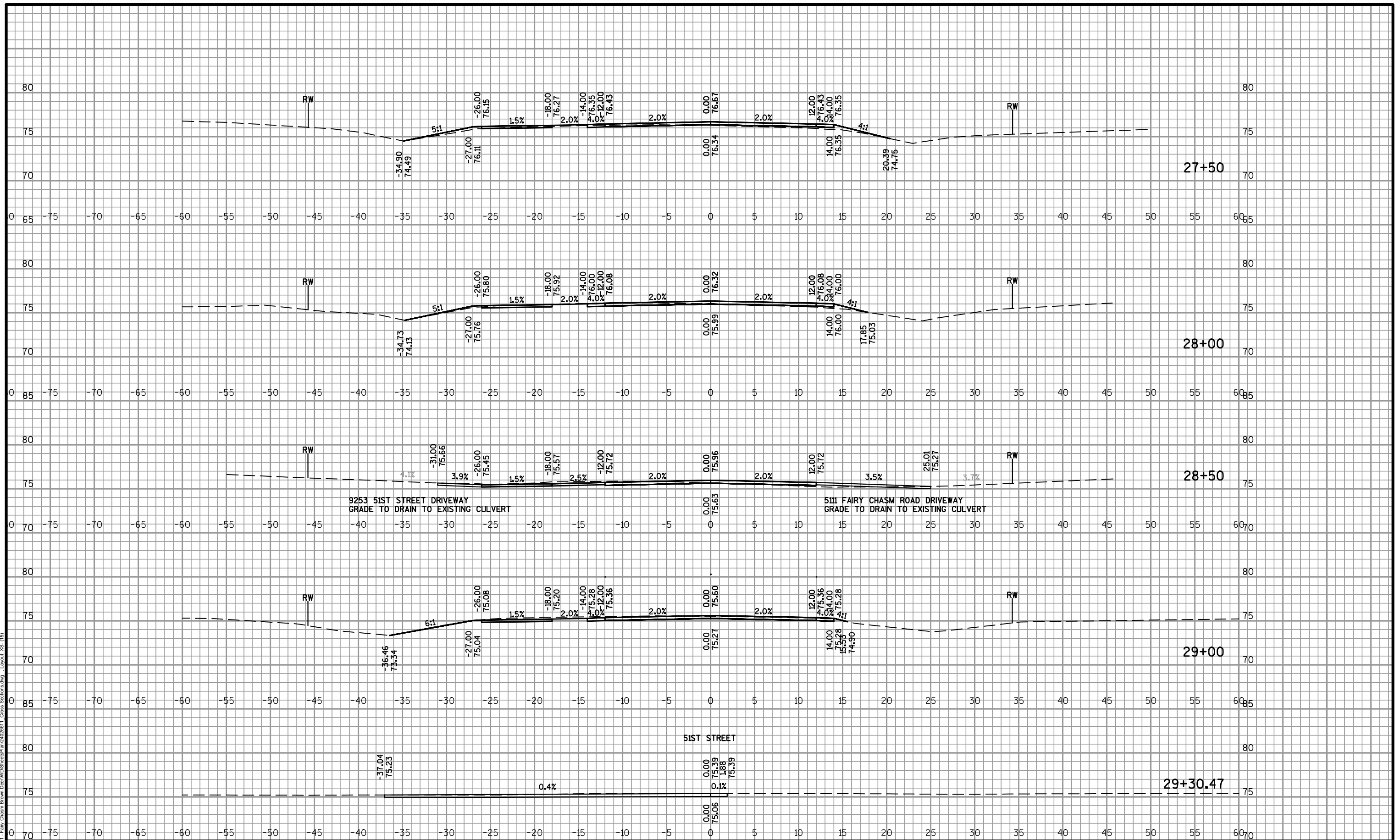
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W. FAIRY CHASM ROAD RECONSTRUCTION
N. 60TH STREET TO N. 51ST STREET
VILLAGE OF BROWN DEER



CROSS SECTIONS

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